

Library

A library (L) facility is centrally located in the town center to provide literary, educational, and informational resources to the community.

Religious Site

The Religious Site (REL) designation is reserved for religious organization facilities operated for worship or promotion of religious activities, such as churches, synagogues, and temples. Religious sites also include religious accessory uses on the same site, including living quarters for staff, child day care facilities, and other uses normally associated with a religious facilities (e.g., schools and recreation facilities), as approved by the Planning Director.

Sites made available for religious uses other than the religious sites designated in Figure 3.1, “Land Use Diagram,” may be approved through a Minor Use Permit and shall be identified on tentative maps.

Transit Station

A multi-modal transit station or terminal (T) is provided in the Plan Area within the east village center with access provided from Watt Avenue. The station/terminal will serve to distribute information on local transit options and serve as a passenger terminal and transfer station for public mass-transit systems including a future, potential bus rapid transit line along Watt Avenue.

Utility Substation

The utility substation (SS) designation allows for electrical substations, pumping stations, pressure regulation stations, or similar facility required to serve the Plan Area.

SCHOOLS:

6 elementary schools, 2 middle schools, and 1 high school are designated by the ES, MS, and HS symbols on the Land Use Diagram. The Land Use Diagram designates 167 acres for schools. School sites are situated adjacent to park sites and open space to allow for joint use of facilities, trail access, and efficient use of the land. Joint school/park sites are centrally placed within each neighborhood to provide a focus for neighborhood interaction and to allow children to walk to school. School sites have been located based on the estimated number of students in each surrounding neighborhood and may need to be revised slightly based on actual build-out densities; one of the middle school

sites may also be relocated in accordance with Section 9.2.6-C, consistent with these location parameters. Schools are sized for “stand alone” facilities, which may develop independently of parks.

Policy 3.21 Land Use for Unused School Sites.

If the responsible School District decides that a school site shown in the Land Use Diagram is not needed, residential development will be permitted at the residential density of the predominant land use adjacent to the designated school site. The total number of residential units allocated to the underlying parcel of record as of the date of the adoption of this Specific Plan (see Table 3-3) will not be increased by the readjustment of the land use. The neighborhood park site located adjacent to the excess school site shall remain as indicated on the Land Use Diagram, providing a central focus for the neighborhood.

If subsequent to the development of the property adjacent to a school site, the School District decides it no longer has a need for the property, it will be necessary to amend the Specific Plan to define the appropriate use of the property.

Elementary Schools

Elementary schools (ES) serve kindergarten through grades 5 or 6 (depending on the school district). Elementary school sites are 12 acres in size with an adjacent neighborhood park. They are located within residential neighborhoods, within easy walking distance (1/2 mile radius), and provide easy auto access from local residential and collector streets.

Middle Schools

Middle school (MS) sites are planned for 22.5 acres in size and will serve grades 6 through 7 or 8 (depending on the school district). 2 middle schools are provided. These are located within residential neighborhoods on major collector streets.

High School

The High School (HS) site, located on Palladay Road between A Street and Base Line Road, is 50 acres. It is planned to serve grades 9 through 12 and is sited on collector streets, in proximity to the surrounding residential community, local commercial services, and adjacent to park facilities located in the community park.

LAND USE

SPECIAL PLANNING AREA:

The Special Planning Area (SPA) designation is located on approximately 979 acres at the western portion of the Plan Area and includes the existing Riego area. There are approximately 150 existing homes within the SPA. Approximately 200 (or 87 percent) of the 230± existing parcels within the SPA are 5 acres or less in size, with the majority being less than 2 acres and located primarily in the Riego township area. The remaining approximately 30 parcels range in size from 5 to 96 acres and are generally located in the vicinity of Newton Street, south of Browning and Colburn Streets. Refer to Figure 3.2 for a map of the SPA.

Of the 14,132 units within the Plan Area, a total of 411 total units are reserved in the SPA for the eventual build-out of this area. These 411 units include the 150 existing homes, leaving an additional 261 new homes allowed for development in the SPA. The 261 additional units reserved for the potential build-out of parcels within the SPA area include: 63 new units allowed to develop consistent with their current zoning and 198 units allowed to develop under the Placer Vineyards Specific Plan and the Specific Plan EIR on a first come, first serve basis. Property owners who choose to subdivide their properties beyond that allowed under current zoning in the SPA will be required to amend this plan (refer to Section 9.2.6).

The main trunk lines of the Placer Vineyards infrastructure system (water, wastewater, and drainage systems) will be sized to serve the anticipated build-out of up to 411 total units reserved within the SPA. Property owners of the SPA may connect to infrastructure systems in the Plan Area but shall be responsible for the costs incurred from the extension of services to their property, including any hook up fees, Plan Area, or Special District fees.

Policy 3.22 Special Planning Area.

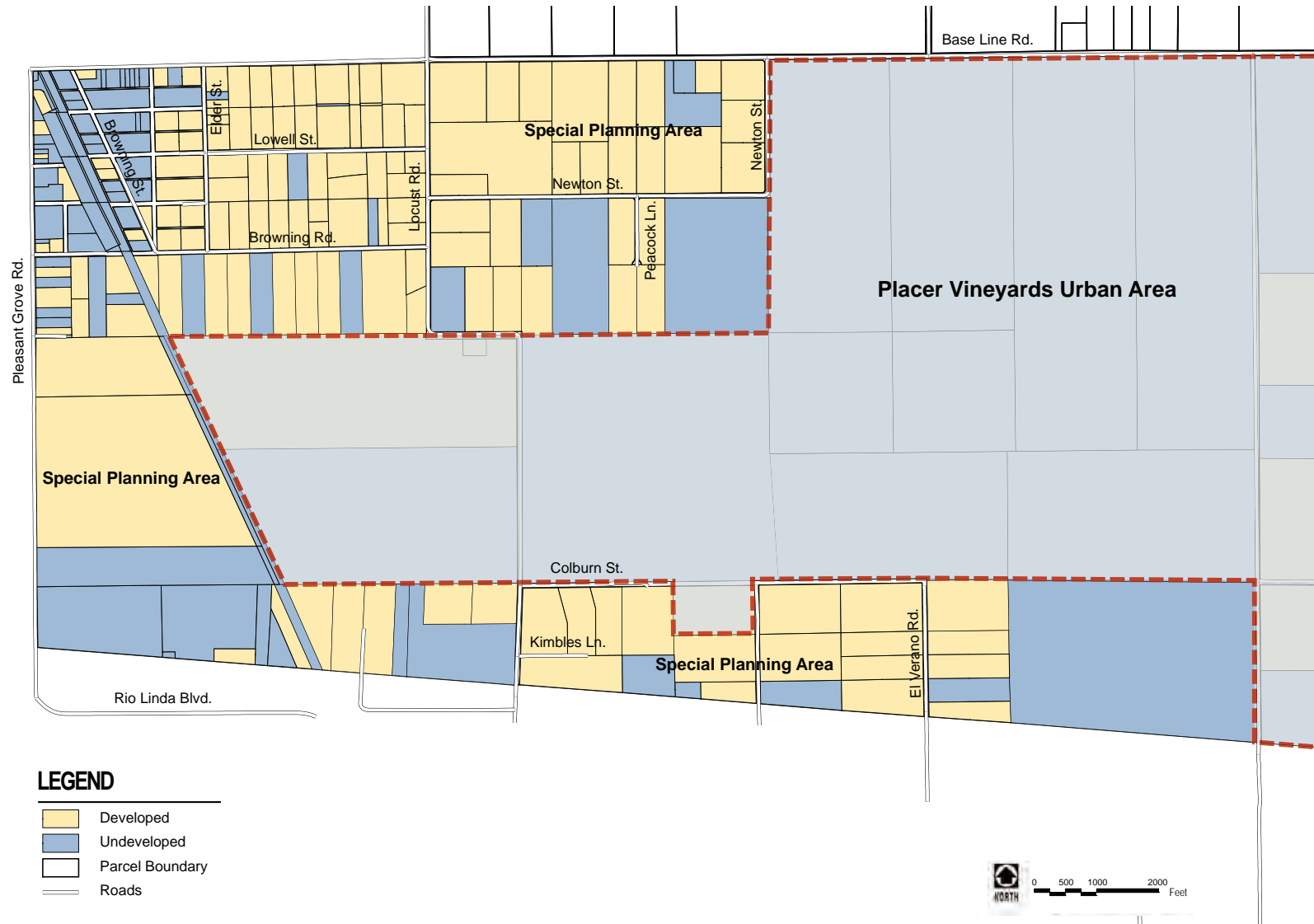
This Specific Plan does not revise or designate zoning for the SPA properties. The corresponding existing zoning for these SPA properties and existing County administrative processes shall govern. Refer to Chapter IX, Section 9.6-D, "Entitlements in the Special Planning Area" for the procedures for requesting entitlements in the SPA.

3.4 LAND OWNERSHIP AND LAND USE ALLOCATION

Figure 3.3 and Table 3-3 summarize the development program for each property owner in the Plan Area. Within the development program, the following standards shall apply.

1. No property may exceed the permitted density range for the land use designation or the allowable number of residential units and commercial intensities assigned to the property in Table 3-3 without a rezoning, Specific Plan amendment, and additional environmental review. However, to provide development flexibility, development transfers between land use parcels is permitted, provided that they meet the criteria set forth in "Density Transfers," described in Chapter IX, "Implementation."
2. When constructed at the same time as the primary residential unit is being constructed on a lot, a secondary dwelling shall be counted as a residential unit for the purposes of calculating the total number of units allowed for a property under Table 3-3. Each such secondary dwelling shall be subject to payment of all fees payable by the primary unit, including those required under the applicable provisions of the development agreement. Lots where secondary dwellings are proposed to be constructed shall be identified as part of any application for the tentative subdivision map for the property.

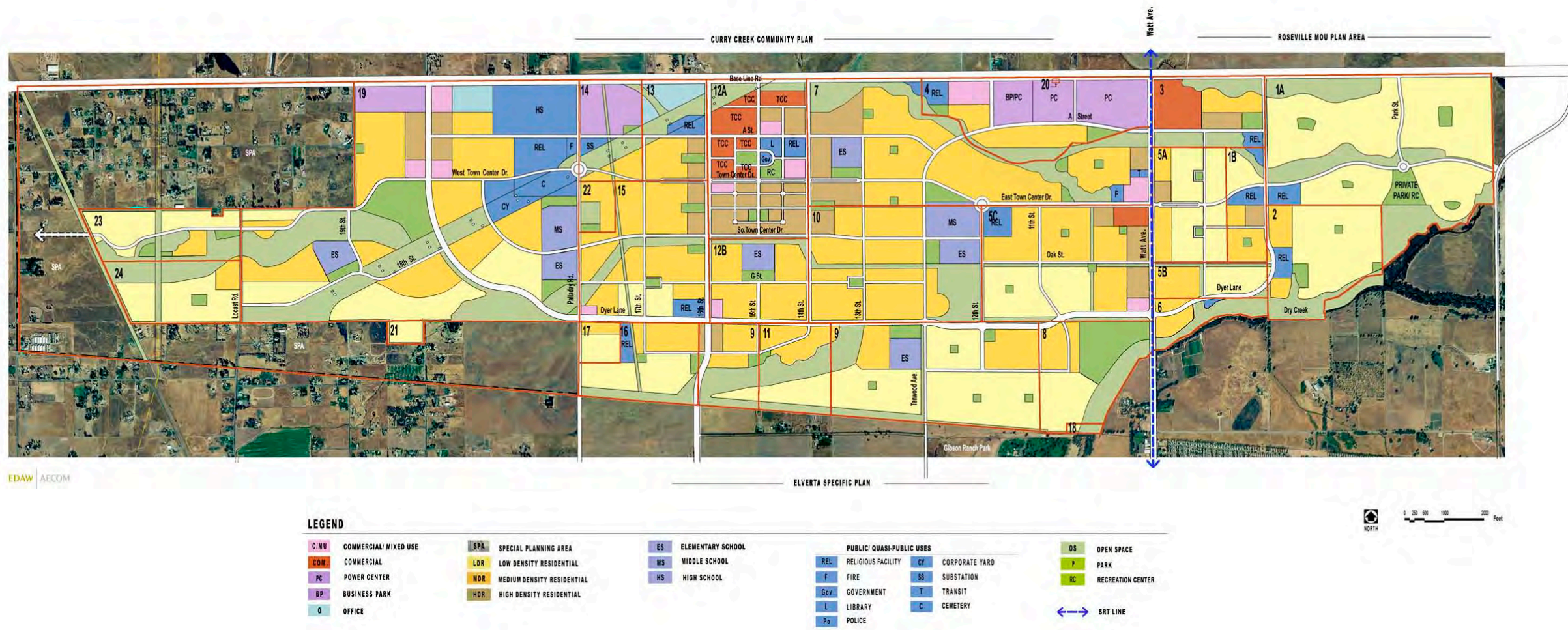
Figure 3.2 Special Planning Area Location Map



Source: Placer County Planning 2005

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Figure 3.3 Land Use Ownership Diagram



LAND USE

Table 3-3 Land Use Ownership Summary

Property ID#	AP#	Property Owner	Gross Parcel Acreage	SPA		LDR 2-6 du/ac		MDR 4-8 du/ac.		HDR 7-21 du/ac.		COM.	OFF	CMU(2) 14-22 du/ac.		BP/PC(3)	Pub	11(4) REL Sites	Schools			Parks(5)	OS	Major Roads	Total Units
				Acres	Units	Acres	Units	Acres	Units	Acres	Units	Acres	Acres	Acres	Units	Acres	Acres	Acres	ES Acres	MS Acres	HS Acres	Acres	Acres	Acres	Units
1A	023-221-001, 002; 023-200-005	Doyle/TR et al; R&B Investments, LLC	402.0			264.0	931											8.0				22.0	88.0	20.0	931
1B	023-200-006	Hodel Family Enterprises, LP	56.0			10.0	35	18.0	128	6.0	90							9.0				2.0	4.0	7.0	253
2	023-200-017	Mourier Family Revocable Lifetime Trust	138.0			82.5	289	21.0	115									6.0				5.0	19.0	4.5	404
3	023-200-037	Auburn & Van Maren, LLC	100.5					26.5	153	7.0	105	25.0						4.0				4.0	26.0	8.0	258
4	023-200-064, 065	Placer 536, LP; B&W 60, LLP	179.2					38.0	225					7.0	88	90.7		7.0				6.0	20.0	10.5	313
5A	023-200-062	Richard Riolo	106.5			66.0	230	24.5	106	5.0	75											3.0		8.0	411
5B	023-200-063	Riolo	51.0			21.0	74	20.0	103														5.0	5.0	177
5C	023-200-015, 028	Riolo, LP	241.5			71.5	250	101.5	562	25.0	375	9.0		4.5	57			9.5				4.0	6.5	10.0	1,244
6	023-200-018	Dana-Yeck Survivor Trust	39.0					14.5	102								1.5						18.0	5.0	102
7	023-200-045, 066	Woodside Reynen & Bardis 356, LP	357.0					169.5	912	46.5	698			6.5	82		4.5	1.5	12.0	2.5		23.0	63.0	28.0	1,692
8	023-200-041	Spinelli Investments, LP Millspin Investments, LP	120.0			32.0	112	25.5	117													32.0	24.5	6.0	229
9	023-200-010, 012, 013	Placer County Land Investors, LLC	326.0			145.0	515	56.5	359	8.0	120								12.0			13.0	61.5	30.0	994
10	023-200-009	Dyer Lane, LP	242.0					148.0	819	10.5	158								12.0	20.0		6.5	30.0	15.0	977
11	023-200-011	P.G.G. Properties, GP	79.0			23.0	81	25.0	134													1.5	27.5	2.0	215
12A	023-200-067	Il Centro, LLC	196.0							57.0	871	42.5		11.5	144		7.0	5.0				11.5	20.0	41.5	1,015
12B	023-200-068	Danville Land Investments, LLC	102.0					64.0	342					3.0	38				12.0			4.0	6.5	12.5	380
13	023-010-024; 023-200-060	Cabral, et al	80.0					20.0	122	7.0	105		17.5					7.5				3.0	13.0	12.0	227
14	023-010-026	D.E. Properties, Inc.	80.0					12.0	58							35.0	4.5					1.0	20.0	7.5	58
15	023-010-004, 029; 023-200-008	Palladay Greens, LLC	202.0			20.0	70	117.0	639	10.0	150			3.0	38			7.0				6.5	23.0	15.5	897
16	023-010-006, 014	Placer 88 Investments, Inc.	94.0			43.0	151	20.0	126									5.5				4.0	16.0	5.5	277
17	023-010-013	J.A. Sioukas Family Partners, LP	19.5			12.0	42	7.5	38																80
18	023-200-042	Mamood Nasserri	3.5			1.0	4																2.5		4
19	023-010-021, 022, 023; 023-150-026, 027; 023-180-005, 006, 007, 008	Baseline A&B Holding, LLC; Lennar Winncrest, LLC	816.5			98.0	343	213.5	1,148	23.0	345		15.0	15.0	189	23.5	33.0	21.0	24.0	22.5	50.0	50.0	159.5	68.5	2,025
20	023-200-029	Jack Garfield	0.3													0.3									0
21	023-019-016	Pandeleon	10.5			10.5	37																		37
22	023-010-028	Slight	22.5					14.0	73													2.0	6.5		73
23	023-160-011	PMF5C, LLC	92.5			49.5	173	8.5	41													5.0	22.5	7.0	214
24	023-160-004	Pandeleon, et al	94.0			52.0	182	11.0	52													2.0	26.5	2.5	234
SPA	Various	Various	979.0	979.0	411																				411
Totals			5,230.0	979.0	411	1,001.0	3,519	1,176.0	6,474	205.0	3,092	76.5	32.5	50.5	636	149.5	50.5	91.0	72.0	45.0	50.0	211.0	709.0	331.5	14,132

Table 3-3 Notes:

- (1) Acres are approximations and subject to change with more detailed mapping, final alignment of roadways, and etc.
- (2) C/MU units are calculated at 70% area coverage.
- (3) 31 acres may be a BP or PC land use (See Figure 3.1, Land Use Diagram, for BP/PC site location.)
- (4) 3 acres of Religious Site are located under power lines.
- (5) 22 acres of parks found in the active adult community (Property #1A) are private parks.

LAND USE

3.5 LAND USE ANALYSIS

3.5.1 POPULATION SUMMARY

At final build-out, the population of Placer Vineyards is estimated to be approximately 32,814 residents. Table 3.4 is a summary of the number of residential unit types and an estimate of the total population of the Plan Area at build-out.

3.5.2 HOUSING SUMMARY

Residential Density:

Residential land uses encompass approximately 65 percent of the total property within the Plan Area, of which 18.7 percent is designated as a Special Planning Area (SPA). The distribution of residential unit types is indicated below in Table 3-4.

Residential Development Standards

Residential Development Standards for residential land use designations can be found in Appendix A, “Land Use and Development Standards.” Residential design guidelines are also provided in Chapter VI, “Community Design.”

Table 3-4 Population and Housing Summary

Unit Type	Density Range (du/ac)	Dwelling Units (du)	% Mix of Units	Household Size (population / du)	Estimated Population
Low Density (Active Adult)	2–6	931	6.6%	1.8	1,675
Low Density	2–6	2,588	18.3%	2.5	6,470
Medium Density	4–8	6,474	45.8%	2.5	16,185
High Density	7–21	3,092	21.9%	2	6,184
Commercial Mixed-use	14–22	636	4.5%	2	1,272
Special Planning Area		411	2.9%	2.5	1,028
Total		14,132	100.0%		32,814

Affordable Housing

The Placer County Housing element sets forth policies and programs intended to provide the opportunity for the County to meet its fair share regional housing allocation and to provide an adequate housing supply for County citizens of all income levels. Placer Vineyards will be required to construct 10% of the dwelling units, within the boundary of the project as affordable to very low-, low-, and moderate-income households. The affordable housing program and any affordable housing agreement for the Plan Area shall be determined in accordance with the policies of this Specific Plan, the General Plan Housing Element, State affordable housing laws and requirements, and the approved Development Agreement.

Assuming the development of 14,132 residential units, the project will be required to provide land to accommodate 1,372 units for very low-, low-, and moderate-income households, as distributed in Table 3-5. Refer to Policy 3.27 for standards on distributing affordable housing units.

For the purposes of this plan housing affordability is based on household incomes defined by the State of California Housing and Community Development (HCD). Very low income households have incomes no greater than 50% of the Placer County median income. Low income households have incomes between 51% and 80% of the Placer County median income. Moderate income households have incomes between 81% and 120% of the Placer County median income. Incomes are adjusted by household size. The HCD determines the median income for Placer County annually.

Affordable Housing Goals

Goal 3.17 Provide at least 10 percent of the residential units in the Specific Plan Area as affordable housing and distribute affordable residential units throughout the community.

Affordable Housing Policies

Policy 3.23 Affordable Housing Requirement.

In compliance with State Law, new development shall provide at least 10 percent of the total residential units in the Plan Area (1,372 units) at a price affordable to very low-, low-, and moderate income households (see Table 3.5).

Policy 3.24 Affordable Housing Obligation.

Owners of residential land will be required to satisfy the affordable housing obligation by constructing a minimum of 10 percent of the units for occupancy by very low-, low-, and moderate-income households, concurrent to and in proportion with development of market rate units within the balance of the property, as defined in the Development Agreement.

Table 3-5 Summary of Affordable Housing Obligation

Affordability Category	Required Allocation (Percentage of Total Units)	Number of Affordable Units Required
Very Low Income	4%	549
Low Income	4%	549
Moderate Income	2%	274
Total	10%	1,372

* These totals represent the affordable obligations of the urban portion of the Plan Area which contains 13,721 homes and does not include the 411 homes allocated to the SPA.

Policy 3.25 Affordable Housing Options.

While individual property owners are responsible for ensuring that land sufficient to accommodate the number of affordable units is available, the Development Agreement shall provide flexibility regarding how the units are provided. Property owners have two options:

- 1) They shall construct the units as part of their market-rate developments generally concurrently and in proportion with the development of market-rate units within the balance of the property, as defined in the Development Agreement, or*
- 2) They shall choose to use one or more of the affordable housing options also specified in the Development Agreement.*

Policy 3.26 Distribution.

Affordable housing units shall be provided as either purchase or rental units, or a mixture of both. For sale/purchase affordable units, including duplex units, may be located anywhere within the Plan Area, provided that the affordable units shall not be clustered together in one location.

Policy 3.27 Concurrent Development of Affordable Units.

Landowners and the County shall enter into Development Agreements that ensures, generally, concurrent development of affordable housing units and development of market-rate units. At the time of approval of the Specific Plan, a Development Agreement will be required to identify the terms for the implementation of affordable housing units. Affordable units shall also be identified on all individual tentative subdivision maps and disclosed to future property owners.

3.5.3 EMPLOYMENT SUMMARY

Table 3.6 estimates potential jobs based on the projected number of employees per acre for office and retail uses. The estimate for school employees assumes 1 employee per 17 students. At full build-out, the Plan Area will provide approximately 7,691 jobs, or approximately 0.5 job per household.

Table 3-6 Employment Summary

Land Use Category	Acres	FAR	Gross Square Feet (GSF)	GSF/Employee	Total Jobs
Retail Uses					
COM: Commercial Retail	34.00	0.25	370,260	500	741
C/MU: Commercial/Mixed-Use Retail (15% of C/MU Total Area)	7.58	0.45	148,485	500	297
TCC: Town Center Commercial Retail (80% of TCC Total Area)	34.00	0.45	666,468	500	1,333
BP: Business Park Retail (10% of BP Total Area)	5.85	0.25	63,707	500	127
PC: Power Center (100% Retail)	60.00	0.25	653,400	500	1,307
BP/PC: Business Park/Power Center ⁽¹⁾ Retail (80% of BP/PC Total Area)	24.8	0.25	270,072	500	540
Subtotal Retail Uses	166.23		2,172,392		4,345
Office Uses					
O: Office	32.5	0.30	424,710	400	1,062
C/MU: Commercial/Mixed-Use Office (15% of C/MU Total Area)	7.58	0.45	148,485	333	446
TCC: Town Center Commercial Office (20% of Town Center Commercial Total)	8.50	0.45	166,617	333	500
BP: Business Park Retail (90% of BP Total Area)	52.65	0.25	573,359	750	764
BP/PC: Business Park/Power Center ⁽¹⁾ Office (20% of BP/PC Total Area)	6.20	0.25	67,518	750	90
Subtotal Office Uses	107.43		1,380,689		2,862
Schools ⁽²⁾	167.00		8,234 students	1 per 17 students	484
Total	440.65		3,553,080		7,691

Notes:

¹ 31 acres may be a BP or PC land use (See Figure 3.1, "Land Use Diagram," for site location).² Refer to school enrollment summary table.

3.7 LAND USE DESIGN AND COMPATIBILITY

Good planning and design can help to increase compatibility between different land uses, reduce potential conflicts, and make for better neighbors. The design and land use pattern of Placer Vineyards is organized to provide an appropriate level of compatibility to adjoining rural residential and agricultural uses as well as between higher intensity commercial and adjoining residential uses.

Compatibility of adjoining land uses is concerned with potential impacts and nuisances that may be created as a result of different levels of activity. Examples of such nuisances may include noise, odors, light and dust, use of chemicals and fertilizers, unwanted traffic and congestion, unwanted views, shade and shadow impacts, and the height, scale and character of the built environment. Creating greater compatibility between disparate land use activities can be achieved through use of appropriate land use patterns and site designs, providing appropriate buffers, or through increased setbacks.

Goal 3.18 Organize the site design and land use patterns in Placer Vineyards to provide an appropriate level of compatibility to adjoining land uses and reduce potential nuisance conflicts.

Policy 3.28 *Compatibility to Adjoining Large-Lot Rural and Agricultural Uses.*

The design and land use pattern of Placer Vineyards shall be organized to provide an appropriate level of compatibility to adjoining rural residential and agricultural uses.

1. *Land use intensities and densities in Placer Vineyards should be stepped down approaching or adjoining rural residential and agricultural uses to create a more compatible transition between uses.*
2. *Open space buffers shall be provided along the entire edge of the Special Planning Area (SPA). Refer to Section 7.3, "Open Space," in Chapter VII, "Parks and Open Space," for the design of open space landscape buffer areas adjacent to the SPA (see Figure 7.10 for required open space buffer setbacks and locations).*
3. *To buffer land in agricultural preserve under the Williamson Act, located north of Baseline Road, a minimum 200-foot setback shall be maintained from the northern existing edge of pavement of Baseline Road for all non-open space and non-infrastructure-related land uses located south of Baseline Road. This 200-foot setback shall only apply to those parcels that are located south of lands that are in active rice farming or located in an agricultural preserve that remains under Williamson Act contract at the time of development. See Figure 3.4 for a map of the location of the Williamson Act agricultural preserve.*

4. *Open space setbacks and surface drainage swales shall be used as a buffer between residential and adjoining agricultural and rural land uses.*
5. *Site design of residential neighborhoods shall employ a number of techniques as a buffer to adjoining uses, including larger lots, larger front or rear yard setbacks, loop roads, cul-de-sacs, single-loaded streets, and open space and landscaped setback buffers. Refer to Section 6.4.1, "General Lot Design," in Chapter VI, "Community Design," for residential lot design guidelines.*
6. *Local streets in new residential neighborhoods shall have limited connections to existing local rural lanes and streets to avoid traffic and parking in adjoining agricultural areas.*
7. *To provide an appropriate level of compatibility between rural residential uses in the Special Planning Area (SPA) and the development of the business park property at the southeast corner of Baseline Road and Newton Street, the following provisions shall apply.*
 - a. *Provide a minimum 115-foot buffer from the west side of Newton Street to the nearest business park building or parking space. This buffer shall consist of a 30 foot distance provided by Newton Street, a 40-foot landscaped berm, and 45-foot on-site building side yard setback. The 45 foot on-site side yard setback shall be landscaped and may include parking (see Figure 7.10 for the conceptual berm and buffer plans and sections).*
 - b. *Loading docks and circulation, including ingress and egress into the Business Park property, shall not be provided from or located adjacent to Newton Street.*
 - c. *Lighting fixtures shall be selected and designed to minimize glare and excess lighting spillage into the SPA. All street and parking lights shall utilize cut-off fixtures to minimize visibility from adjacent areas of the community. Lighting shall not be blinking, flashing, or be of unusual intensity or brightness.*
 - d. *Buildings shall be designed at heights and intensities that provide an appropriate level of compatibility to the SPA. Business Park development projects shall be subject to the requirements of the Design/ Site Review process.*

Policy 3.29 Compatibility of Residential Uses Adjacent to Commercial and Employment Uses.

The design and land use pattern of Placer Vineyards shall be organized to provide an appropriate level of compatibility between residential neighborhoods and adjoining commercial and higher intensity office uses.

- 1. Residential densities in Placer Vineyards should create a transition between commercial and office uses and adjoining low density residential neighborhoods. Transitional uses may include higher density residential uses, public and quasi-public uses, open spaces, and parks.*
- 2. Local streets will avoid direct connections between large commercial centers and surrounding residential areas that could allow commercial traffic to pass through the residential neighborhoods and affect local streets with commercial parking.*
- 3. Open space setbacks, parks, and surface drainage swales are appropriate buffers between residential uses and adjoining higher intensity commercial and office uses.*
- 4. Site design of residential neighborhoods can employ a number of techniques as a buffer to adjoining uses, as indicated in Figure 6.18, "Lotting Conditions Adjacent to Major Roadways," in Chapter VI, "Community Design." Such techniques include larger setbacks, loop roads, cul-de-sacs, frontage roads, single loaded streets, open space, and landscaped buffers.*

Figure 3.4 Map of Williamson Act Parcels



CHAPTER IV: ENVIRONMENTAL RESOURCES



4.1 ENVIRONMENTAL RESOURCE CONCEPTS

Context

This chapter identifies the environmental conditions and sensitive resources found in the Plan Area. Goals and policies contained in this Specific Plan shall guide the conservation, protection, or mitigation of existing environmental conditions and sensitive resources. This chapter addresses seven key areas: wetland resources, special-status species, oak and riparian woodlands, water quality and conservation, energy conservation, air quality, and noise.

The existing environmental conditions present in the Plan Area were taken into account during development of the Land Use Plan. The Land Use Plan is designed to protect significant, sensitive resources in open space and to minimize the impacts of urban development on the existing and natural communities in the Plan Area, to the extent feasible. This Specific Plan, however, recognizes and provides off-site mitigation of environmental resources. Through this approach of on- and off-site environmental mitigation, the potential to protect large areas of significant open space is maximized.

Goal 4.1 Establish a comprehensive approach for the replacement of affected open space and agricultural and habitat areas.

Placer County Conservation Plan

Placer County is currently preparing a Natural Community Conservation Plan and Habitat Conservation Plan to address the conservation of natural communities, endangered species, and other less sensitive species of native wildlife. At the same time the County is pursuing a California Department of Fish and Game (DFG) Master Streambed Alteration Agreement and Clean Water Act Section 401 water quality certification. Collectively, these activities have been termed the Placer County Conservation Plan (PCCP). The County is also in the process of applying for a Clean Water Act Section 404 Programmatic General Permit through a County Aquatic Resource Permit (CARP).

The purpose of the PCCP will be to encourage and simplify the process of conserving sensitive habitats for special-status species. The Placer County General Plan and the PCCP will set the minimum mitigation ratios needed for the off-site mitigation component of the eventual federal, state, and local mitigation package. All projects designed during the preparation of the current first phase of preparation of the PCCP are to be consistent with the principles and objectives of the conservation process. Because activities related to implementation of the Specific Plan may commence before the approval of Phase 1 of the PCCP, this Specific Plan and the mitigation measures included in the project Environmental Impact Report (EIR) are designed to be implemented before approval of the PCCP.

Placer Vineyards Open Space Mitigation and Management Plan

The property owners within the Plan Area have developed a strategy and program designed to reduce the impacts of the Specific Plan on open space and biological and agricultural resources. The intent of this mitigation strategy is to provide a single, all-inclusive mitigation measure designed to simultaneously address possible impacts on all biological resources of concern, while also addressing potential impacts on open space and agricultural lands. The goal in devising this strategy is to formulate a biological protection, preservation, and mitigation program that includes measures likely to find their way into the PCCP, while also mitigating impacts to open space and agricultural lands. The mitigation strategy is formulated to simultaneously satisfy the requirements of CEQA, the Placer County General Plan, and other federal, state, and local statutes, regulations, and policies that affect open space, agricultural lands, and biological resources. The program also seeks to strike a reasonable balance between on-site resource avoidance and off-site preservation and restoration.



To address the need for replacement habitat, agriculture, and open space areas, 6 potential off-site mitigation sites have been identified for the project. Placer Vineyards property owners may either choose to acquire land located in the 6 mitigation sites, or alternate mitigation sites may be acquired that would achieve comparable mitigation ratios. All off-site mitigation must be in accordance with the terms of the PCCP, once it is approved, or as permitted by the U.S. Army Corps of Engineers (USACE), U.S. Fish and Wildlife Service (USFWS), and DFG.

The property owners' proposal also requires that a combination of one or more mitigation sites establish a core preserve area of approximately 1,000 acres set aside and protected by permanent conservation easement before the initiation of any ground-disturbing activities. The remaining mitigation requirements, beyond the 1,000 acres of open space required to be set aside, are to be addressed on an incremental basis as the development of individual projects proceed. At the time of establishment of the preserves, at the time of additions to preserves, and/or at the time of incremental additions to the geographic scope of a preserve, a management plan for the operation of the preserve lands must be approved by the County.

ENVIRONMENTAL RESOURCES

Mitigation areas may be discontinuous with one another as a result of land additions to preserve holdings so long as the land holdings meet the minimum acreage and location requirements of mitigation measure 4.4-1 in the Specific Plan EIR. Those requirements include a minimum area of 200 acres and location within the General Plan's Agricultural and Open Space categories. Subsequent Specific Plan projects shall mitigate (for agriculture, wetlands, and/or habitat) through the establishment of preserve areas that, to the extent feasible, are located adjacent to the core preserve or other existing preserve sites.

Policy 4.1 *Open Space Mitigation and Management Plans.*

One acre of open space will be preserved within Placer County for each acre of open space affected within the Specific Plan area. The process through which this will be accomplished will be the approval and implementation of a series of Open Space Mitigation and Management Plans that address the management of a specific property to be preserved for mitigation of lost open space, agricultural land, and habitat. An Open Space Mitigation and Management Plan will be required for each individual development project or grouping of projects within the Specific Plan area.

4.2 WETLAND RESOURCES

Wetland resources include various types of water features such as creeks, drainage basins, lakes, ponds, marshes, and seasonal wetlands. Wetlands can also be perennial and seasonal water features. Wetlands are an important part of the environmental condition because wetlands support a variety of sensitive wildlife and plant species.

Two broad categories of wetlands are identified in the Plan Area: Depressional wetlands and Riverine wetlands. Depressional wetlands include vernal pools, seasonal wetlands, seasonal marshes, seasonal drainage, and ponds. Riverine wetlands include a perennial drainage (Dry Creek) and ephemeral/intermittent drainage.

Other waters also located in the Plan Area are stock ponds, channels, and ditches. Refer to Figure 4.1 for an illustration of the location and extent of existing wetlands. See the Placer Vineyards EIR for a more detailed description and representation of the existing wetlands. Additional detailed analysis will be required for individual project sites with the submittal of development proposals.

4.2.1 WETLAND TYPES

Descriptions of wetlands found in the Plan Area are provided below.

Depressional Wetlands

Vernal Pools:

Vernal pools are shallow depressions with an underlying hardpan layer that causes them to inundate. Vernal pools typically support plant species found in the Sacramento Valley.

Seasonal Wetland:

Seasonally inundated basins are scattered throughout the Plan Area. These areas may be categorized as seasonal wetlands depending on their floristic composition and hydrology. Seasonal wetlands can be found within swales, drainages, or depressions and typically support wetland plants, including grasses, native plants, and nonnative forbs.

Seasonal Marsh:

Seasonal marshes occur when water remains in an intermittent drainage long enough that marsh vegetation becomes established. Riparian habitat occurs along the southeastern edge of the project adjacent to Dry Creek.

Seasonal Drainage:

Seasonal drainage conveys water during the rainy season and becomes dry during the remainder of the year. Seasonal drainage is typically inundated during storm events and remains saturated into the vegetation growing season.

Ponds:

Several large hydrologic features are present in the Plan Area, consisting of bermed swales, natural basins, and artificial, excavated stock ponds. Ponds support little, or a fringe of, perennial vegetation, dominated by cattail, tule, and common rush.

Riverine Wetlands

Perennial Drainage:

Dry Creek runs along the Plan Area's southeastern boundary and has water flowing year round. Vegetation such as Goodding's Willow, Fremont's cottonwood, Valley Oak, and herbaceous species grow along the banks of Dry Creek.

Ephemeral/Intermittent Drainage:

Two types of ephemeral drainage features are found in the Plan Area: intermittent drainage and drainage swales. Intermittent drainages can be several feet wide and are typically devoid of vegetation because of fast-moving water. Drainage swales are hydrologic linear features and have floristic characteristics similar to seasonal wetlands, including non-native grasses and forbs.

4.2.2 WETLANDS AVOIDANCE AND PRESERVATION

One of the first steps in designing the Land Use Diagram was to identify existing environmentally sensitive areas, including wetlands. Responding to the mapping of sensitive areas, the Land Use Diagram designates sites along natural drainage corridors as open space areas, including wetlands and other environmentally sensitive areas.

The intent of this approach is to preserve the highest quality and most contiguous wetlands within open space areas and at sites along natural drainage corridors. Long-term wetland preservation may also better be achieved in large off-site preserves, which avoid the urban edge effects associated with preserved wetlands in the midst of an urban area.

The final design of wetland corridors and open space areas will be determined during site-specific development proposals and associated site-specific wetland delineations. Although not reflected within the Land Use Diagram, it is anticipated that lands containing off-site wetlands and open space will also be preserved in perpetuity as an element of plan implementation.

Goal 4.2 Avoid and minimize adverse impacts on wetlands to the extent feasible.

Policy 4.2 Individual development projects shall, to the extent feasible, develop plans that will preserve and protect existing wetland areas.

Goal 4.3 Develop a plan for mitigation of disturbance of on-site wetlands.

Policy 4.3 Where wetland avoidance is not feasible, a wetland mitigation plan will be developed before site disturbance to mitigate all wetland impacts. Mitigation plans will be prepared in accordance with all state and federal regulations and in conjunction with the request for permits from regulatory agencies.

Policy 4.4 Wetland feasibility studies will be prepared to ensure successful establishment of the compensation wetlands in conjunction with the request for permits from regulatory agencies.

Policy 4.5 Compensation wetlands will be constructed within designated open space areas of the Plan Area, or wetland mitigation credits will be purchased from the USFWS approved mitigation bank, or land at an off-site location will be purchased to preserve and or construct mitigation wetlands, or a combination of these alternatives will be implemented.

Goal 4.4 Establish a maintenance and monitoring program to ensure that wetland compensation areas are protected and mitigation measures are successful.

Policy 4.6 Maintenance and monitoring of wetland compensation areas will be conducted in accordance with requirements of USACE and pursuant to the issuance of a Section 404 permit.

Policy 4.7 Maintenance of wetland compensation areas will be the responsibility of a County Service Agency (CSA) or other funding mechanism satisfactory to USACE and Placer County.

Policy 4.8 Monitoring of the efficacy of the mitigation program will comply with federal agency requirements and the California Clean Water Act and DFG Code, Streambed Alteration Agreements, and all DFG provisions.

Policy 4.9 Maintenance and monitoring programs will be required for compensation wetlands purchased in mitigation banks.

Goal 4.5 Provide development plan features that will ensure the long-term health of wetland areas.

Policy 4.10 Only passive recreation activities compatible with natural communities will be allowed in wetland preserve areas.

Policy 4.11 Hunting, dumping, operation of motorcycles, or any other activities that could be detrimental to the wetland ecosystems are strictly prohibited.

Figure 4.1 Wetland Diagram



Source: ECORP 2005

4.3 SPECIAL-STATUS SPECIES

The Plan Area incorporates habitat that support special-status species. The Land Use Plan designed for the Plan Area attempts to avoid potential development impacts on sensitive species by preserving habitat areas, where feasible. Site-specific biological surveys, conducted as part of individual project proposals, will verify existing habitat and species types.

4.3.1 SPECIAL-STATUS SPECIES HABITAT AVOIDANCE

Existing environmentally sensitive areas were identified during the design of the Land Use Plan for the Plan Area. As a result, the Land Use Plan designates numerous open space areas and sites around environmentally sensitive areas.

The overall intent of this approach is to preserve existing on-site sensitive habitat areas, to the extent feasible. However, impacts of development on existing sensitive habitats may be unavoidable, in which case off-site mitigation lands will be provided. The final configuration and design of open space areas to preserve sensitive habitats and the extent of off-site mitigation measures will be determined during site-specific development proposals and associated site-specific biological surveys.

Goal 4.6 Identify potential special-status species habitat areas and mitigate impacts on these areas.

Policy 4.12 *Biological surveys will be conducted to identify potentially occurring special-status species before disturbance of habitat areas and in conjunction with requests for permits from regulatory agencies.*

Policy 4.13 *Where special-status species habitats are indicated, project-specific mitigation measures will be developed in consultation with Placer County, DFG, and/or USFWS.*

Policy 4.14 *Where state or federally listed special-status species may be adversely affected, required consultation will be conducted and/or appropriate permits obtained before disturbance of habitat areas.*

4.4 OAK AND RIPARIAN WOODLANDS

The Plan Area historically involved grazing and dry land farming. Two large stands of valley oaks still remain in the east and central portions of the Plan Area. Oak trees grow along Dyer Lane and riparian woodlands are found along Dry Creek. The Land Use Plan for the Plan Area designates open space uses to preserve the oaks and riparian woodlands.

4.4.1 TREE PRESERVATION

All oaks and riparian woodlands located in the Plan Area are subject to the Placer County Tree Preservation Ordinance and the policies of this Specific Plan. Measures provided in this Specific Plan apply to both private developments and public projects.

Goal 4.7 Preserve oak trees and riparian woodlands.

Policy 4.15 *Oaks and other native trees with trunk diameters of 6 inches or greater, measured at breast height, will be preserved wherever feasible.*

Policy 4.16 *Location and preservation of oaks and other native trees will be indicated on site-specific, tentative maps.*

Policy 4.17 *Mitigation for trees removed from existing riparian or improved drainage corridors will be accomplished according to the following procedures:*

- ♦ *For each riparian tree removed, one 15-gallon tree, one 2-inch by 10-inch tube container (Deepot 40) seedling for each inch of diameter of the removed tree, and three 1-gallon shrubs will be planted in the riparian or improved drainage corridors.*
- ♦ *For each oak tree greater than 6 inches diameter at breast height that is removed, one 15-gallon tree, one 2-inch by 10-inch tube container (Deepot 40) seedling for each inch of diameter of the removed tree, and three 1-gallon shrubs will be planted.*

Policy 4.18 *Site-specific design and tree preservation, removal, and mitigation will be identified on an individual project basis and shall conform to the requirements developed in the Mitigation, Monitoring, and Reporting Program.*

Policy 4.19 *During construction, brightly colored, temporary plastic fencing that is at least 4 feet tall will be erected 1 foot outside the outermost edge of the tree's dripline or around the combined dripline of groves or lines of trees for protection. Signs will be erected in accordance with the Placer County Tree Preservation Ordinance.*

ENVIRONMENTAL RESOURCES

Policy 4.20 *Soil disruptions within driplines of existing oaks and other native trees identified for preservation will be avoided where feasible. Paving shall not be placed in the driplines of trees to be preserved.*

Policy 4.21 Grading and landscaping will be designed to prevent overspray or runoff within tree driplines.

Policy 4.22 *Irrigation will not occur within the driplines of indigenous oaks, except as prescribed by the project arborist or landscape architect.*

Policy 4.23 *Plantings within driplines of indigenous oaks will be limited to species that require no irrigation and are tolerant of the natural semiarid habitat of the oaks.*

4.5 WATER QUALITY AND CONSERVATION

Development within the Plan Area could potentially have adverse impacts on water quality, including short-term impacts from construction activities and long-term impacts from urban runoff. The Specific Plan minimizes potential water quality impacts through the preservation of existing drainage ways and establishing specific locations for detention and water quality basins and best management practices (BMPs) to contain, retain, and filter storm water runoff.

Goal 4.8 Reduce water quality impacts within the Plan Area to the maximum extent practicable.

Goal 4.9 Site-specific development projects should incorporate low-impact development design principles into the site layout.

Policy 4.24 *Storm water Quality Improvements.*

Storm water management improvements disbursed through the Plan Area provide treatment to runoff before it enters the natural drainage conveyance systems in open space areas. In addition, by integrating the storm water management system throughout the Plan Area, individual parcels can provide specific storm water management elements that respond to the particular site conditions. This will promote the removal of various potential pollutants from each parcel before they are discharged into the drainage system. The following standards will apply to development projects.

1. *During construction, BMPs shall be provided to stabilize soils in place and minimize the amount of sediment entering the storm drain system and drainageways. BMPs shall generally consist of a combination of the following measures: minimizing soil disturbance, hydroseeding, fiber rolls, inlet protection, and stabilized construction access.*

2. *After construction, regional water quality facilities identified in the Master Project Drainage Report shall be constructed concurrently with the backbone drainage infrastructure for permanent water quality treatment.*

3. *Development projects shall provide on-site, site-specific post-construction water quality treatment facilities to capture and remove the pollutants before they are discharged from the site. Water quality treatment facilities shall generally consist of a combination of the following measures: vegetated swales, infiltration trenches/basins, filter strips, sand/oil separators, trench drains, and porous pavement.*

Policy 4.25 *Low-Impact Development Design.*

To the extent feasible, site-specific development projects shall incorporate low-impact development design strategies that may include:

1. *Minimizing and reducing impervious surfaces of site development (eg., roadways, sidewalks, driveways, parking areas, and rooftops);*
2. *Breaking up large areas of impervious surface and directing flows from these areas to stabilized vegetated areas; and*

4.5.1 GROUNDWATER RECHARGE

Groundwater supplies are recharged by rainwater that percolates through to the saturated zone of the soil. The principal means of groundwater recharge in the Sacramento Valley is accomplished within the streambeds that cross the valley. The rate and quality of rainwater reaching the saturation zone depends on a number of factors, such as the amount and duration of precipitation, soil type, moisture content of the soil, and vertical permeability of the unsaturated zone. Urban land uses also affect groundwater recharge by reducing the amount of permeable surface, which limits the amount of rainwater able to percolate into the soil. In addition, groundwater levels can be affected by pumping for agriculture and urban uses.

The entirety of the creek system will remain in place and will continue to serve as the principal means of groundwater recharge in this area. The land use design prepared for the Plan Area aims to reduce the effects of urbanization on groundwater recharge by designating 709 acres (13.6% of the Plan Area) as open space and preserving existing drainage ways.

Goal 4.10 Conserve and preserve water quality within the Placer Vineyards Plan Area.

Policy 4.26 *Site grading will be undertaken and controlled so that sediment runoff is minimized. In locations approved by the County, detention basins may be located in open space areas so as to minimize increases in peak flows from the site. The basins may facilitate groundwater recharge, but to a limited degree because of the predominance of clay soils in the area. To minimize runoff, paved parking areas will be designed to provide the minimum amount of paving area necessary to meet required parking and circulation standards. The following standards apply to site-specific development projects.*

1. Storm water management plans will be prepared that comply with all standards and requirements of the National Pollutant Discharge Elimination System (NPDES) and the grading, erosion, and improvement standards in the Placer County Storm water Management Plan.
2. Grading plans submitted for Placer County review and approval will contain an erosion and sediment control plan that includes erosion control measures to protect waterways from erosion and debris during and after construction activities.
3. Grading plans will be designed to minimize run-off and the area of disturbance during construction.
4. A Storm Water Pollution Prevention Plan (SWPPP) will be prepared and implemented for site-specific projects.
5. Concurrent with construction of site improvements, storm water BMPs will be constructed and maintained in accordance with the SWPPP as approved by the Central Valley Regional Water Quality Control Board.
6. Drainage reports will be prepared for site-specific projects and shall comply with the Placer County Storm Water Management Manual, Land Development Manual, and Placer Vineyards Master Project Drainage Report.

Policy 4.27 Impacts on Streams and Riparian Areas.

Impacts on Curry Creek and Dry Creek shall be avoided through the conveyance of open space buffers and the location of adjacent land uses. A 100-foot setback shall be provided for perennial streams (Curry Creek). The majority of the active floodplain of Dry Creek lies on the opposite bank and not within the reach of Dry Creek adjacent to the Plan Area. Adequate open space protection

has been designated in the Specific Plan to mitigate impacts on Dry Creek. Implementation of the Specific Plan will also ensure that there would be no net loss of riparian habitat (see Policy 4.17).

Policy 4.28 *Construction of stream crossings or other improvements in the Dry Creek and Curry Creek corridors will be kept to the absolute minimum necessary and will meet the following standards:*

1. A Streambed Alteration Agreement will be obtained from the DFG before commencement of construction of stream crossings.
2. Areas adjacent to finished improvements in Curry Creek and Dry Creek that are disturbed during construction activities will be hydroseeded and revegetated.
3. Disturbed areas in Curry Creek and Dry Creek not actively being developed will be planted, mulched, or otherwise protected by an acceptable means for the duration of the winter.
4. Construction activities will not be permitted within Curry Creek or Dry Creek that would result in disturbed areas being left unprotected between October 15 and March 15.
5. Construction roads crossing creek systems will be used only when necessary and other access routes are not feasible.
6. Construction roads crossing creek systems will incorporate culverts if roads remain in place during the winter season.
7. Erosion control measures will be in place before the onset of the rainy season, but no later than October 15 during construction.
8. Permanent roadway stream crossings will be designed for a 100-year flood event.
9. Stream crossings will be designed with approaches as close to a right angle wherever feasible.
10. Stream crossings will be designed to reduce erosion and stream degradation by the placement of structures.
11. Rock energy dissipaters or other Placer County-approved methods will be used at outflow points of all culverts.

ENVIRONMENTAL RESOURCES

Policy 4.29 *Developers shall provide a schedule of construction projects to Placer County. Placer County will provide inspection of construction sites to ensure that erosion control measures are operative through the winter period.*

Policy 4.30 *Maintenance access easements will be dedicated to Placer County on all portions of open space and stream corridors as identified on the tentative maps of development projects and approved by the County.*

Policy 4.31 *Maintenance access easements will include areas required for preserving trees, special-status species habitat, and wetland resources.*

Policy 4.32 *Use of low-water-consumption plant materials and irrigation systems will be encouraged by Placer County and the following standards will be met and implemented by site-specific development projects.*

1. *Where available and feasible, recycled water will be used to irrigate all parks, schools, and public rights-of-way. Irrigation equipment shall be compatible with the use of reclaimed water.*
2. *Low-volume spray irrigation systems shall be utilized for turf and groundcover areas and drip irrigation systems for shrubs and trees.*
3. *Where recycled water is available, water-intensive landscaping may be used.*
4. *Landscaping in improved common areas will incorporate drought-resistant varieties where practical and consistent with Placer County design guidelines.*
5. *Landscaping within medians should be by subsurface drip irrigation systems.*

Policy 4.33 *Use of currently available water conservation devices will be encouraged by Placer County in all existing development. To accomplish this, Placer County will meter the use of potable water, and new construction must meet the following standards.*

1. *Water-conserving design and equipment will be required in all new construction.*
2. *Recycled water will be used for irrigation where feasible.*

4.6 SOILS

Soils in the Plan Area generally consist of clays, loams, and sandy loams. For additional information regarding soil types and characteristics within the Plan Area, refer to the Placer Vineyards Specific Plan EIR Appendix L.

Soils are categorized by the Natural Resources Conservation Service (NRCS) for their potential use as agricultural land. The California Department of Conservation (CDC) Farmland Mapping and Monitoring program uses NRCS categories to prepare “Important Farmland Maps.” Most of the Plan Area, approximately 3,657 acres of farmland, are characterized as of “Local Importance” (3,401 acres) or “State-wide Importance” (256 acres) by the CDC. Specific Plan policies will preserve open space land elsewhere in Placer County to offset development of these lands.

Goal 4.11 *Minimize impacts on important farmland through the dedication of other open space land.*

Policy 4.34 *Require the preservation of other open space/agricultural lands elsewhere in Placer County at a ratio of 1 acre of land for each acre of land affected within the Plan Area.*

Policy 4.35 *No additional agricultural land dedications shall be required beyond the 1:1 open space dedication required under Policy 4.34 so long as a substantial portion, as determined by County staff, of the open space lands acquired are:*

1. *In agricultural production or have the potential to support agriculture;*
2. *Undeveloped and have an NRCS soils classification of the same or greater value than lands being impacted within the Specific Plan project; or*
3. *Undeveloped and have the same or higher value CDC categorization as lands being impacted within the Specific Plan project property at issue.*

In-kind mitigation is not required for agricultural land developed within the project area.

4.7 ENERGY CONSERVATION

The Specific Plan's land use patterns and transportation systems are designed to encourage efficient energy use through nonmotorized transportation and the close proximity of residential uses to jobs and services.

Goal 4.12 Encourage efficient energy use and conservation.

Policy 4.36 *All residential units will be developed in compliance with State of California Title 24 energy conservation measures.*

Policy 4.37 *Use of passive and active solar devices such as solar collectors, solar cells, and solar heating systems, integrated into the building designs, are encouraged.*

Policy 4.38 *Building and site design should take into account the solar orientation of buildings during design and construction.*

4.8 AIR QUALITY

The Plan Area is located in the jurisdiction of the Placer County Air Pollution Control District (PCAPCD), which is the agency responsible for monitoring and regulating air pollutant emissions from stationary, mobile, and indirect sources within Placer County. Development within the Plan Area could affect air quality in Placer County and the greater Sacramento Valley. Air quality in the Sacramento Valley affects the quality of life for all residents living in the Sacramento region. The primary factors contributing to air quality in the vicinity of the Plan Area include:

- ♦ Climatic variances, temperature inversions, and low wind speeds;
- ♦ Vehicle emissions; and
- ♦ Mobile and stationary pollutants generated by localized urban development.

Vehicle trips are a major contributor to air pollution, and the number of vehicle trips is affected by the spatial design of land uses and destinations within the community. The Specific Plan includes three major features that help reduce or minimize impacts on air quality.

First, the Plan Area includes three centralized mixed-use village cores that provide neighborhood commercial uses to encourage pedestrian/bicycle use between surrounding residential areas and the village core land uses.

Second, the Specific Plan improves the regional balance of housing and jobs. Housing opportunities made available closer to employment encourage fewer long-distance commutes, consistent with the objectives of the SACOG Blueprint Plan. To this end, SACOG has designated the Placer Vineyards Plan Area as a major regional development opportunity to improve the jobs/housing balance.

Third, the land use pattern and transportation system also facilitates the use of alternative transportation choices throughout the Plan Area. The plan provides for a future bus rapid transit route and transit node within the village center along Watt Avenue. The Plan also uses an extensive bike and pedestrian system along roadways and major open space corridors, linking residences to the bus rapid transit system, the village centers and town center, and public facilities. A street pattern of multiple and parallel routes between destinations minimizes traffic congestion and facilitates residents to combine vehicle trips into one route.

Goal 4.13 Minimize air quality impacts on the Placer Vineyards area and the region.

Policy 4.39 *Local area source emissions shall be minimized through a variety of strategies:*

1. *Promote low-emission energy use by requiring building design features that accommodate and encourage use of alternative energy sources.*
2. *Promote low-emission energy use by incorporating landscaping conducive to passive solar energy uses:*
 - a. *Buildings that are oriented in a south-to-southwest direction, where feasible;*
 - b. *Deciduous trees that are planted on the west and south sides of structures;*
 - c. *Landscapes with drought-resistant species and groundcovers rather than pavement to reduce heat reflection; and*
 - d. *Maximum parking lot shading at all non-residential developments.*

Policy 4.40 *Provide, on a project-specific basis, adequate buffers designed to separate emission and nuisance sources from residential uses, consistent with the Placer County General Plan.*

Policy 4.41 *Construction activities will comply with all requirements of grading permits and PCAPCD.*

Policy 4.42 *PCAPCD may replace or supplement air pollution control measures for individual projects as new technology and feasible measures become available over the course of the Plan buildout.*

4.9 VECTOR CONTROL AND MOSQUITO ABATEMENT

The purpose of this section is to provide guidelines that prevent new mosquito breeding sources and to apply the necessary measures that support the agencies involved in the surveillance, management, and elimination of mosquito breeding sources. Vector sources such as creeks, wetlands, vernal pools, and other naturally occurring habitat along with artificial sources—agricultural, industrial, and residential sources—will be routinely checked and monitored for mosquito production.

Goal 4.14 Protect public health and safety by preventing the creation of mosquito breeding areas through proper drainage and routine surveillance of standing water sources for mosquito production.

Policy 4.43 *Grading shall be performed in a manner to prevent the occurrence of standing water or other areas suitable for the breeding of mosquitoes or other vectors. Water detention and related surface water conveyance features shall also be designed to prevent the breeding of mosquitoes.*

Policy 4.44 *The Placer Mosquito Abatement District shall be granted access in perpetuity to perform vector control in all common areas, including drainage, open space corridors, and park areas. Such access shall be a condition of approval of all tentative maps approved within the Plan Area.*

4.10 NOISE

A major source of noise affecting the Plan Area is vehicular traffic along arterial roadways. Other local roadways also contribute to noise in the environment. The Specific Plan will allow for development of residential uses (which are considered to be noise-sensitive users) adjacent to highly traveled roadways. As development of residential land uses occurs in the Plan Area, individual projects will be analyzed for potential noise impacts.

In most cases the edge treatments and orientation of buildings along arterial streets will reduce outdoor noise levels to an acceptable level (60 decibel day-night average noise level, community noise equivalent level [60 dB DNL] or less). In those instances where the noise level is in excess of the 60 dB DNL standards, design practices shall be implemented to reduce noise levels in outdoor use areas to less than 60 dB DNL.

To reduce noise levels, sound walls, berms, and other devices need to be constructed to buffer noise created by vehicles on adjacent residential land uses. However, the Placer County General Plan discourages the use of sound walls for noise mitigation. The General Plan encourages the use of setbacks, building orientation, and other alternatives to sound walls. The General Plan further provides that, where noise mitigation measures are required to achieve adopted standards, the emphasis shall be placed upon site planning and project design. The use of noise barriers shall be considered only after all other practical design-related noise mitigation measures have been integrated into the project. The policies of this chapter and of Chapter V, “Transportation and Circulation,” and Chapter VI, “Community Design,” of this Specific Plan provide the means and methods to satisfy these General Plan requirements.

Goal 4.15 Minimize noise impacts on residential land uses.

Policy 4.45 *Edge treatments and building orientations along arterial streets will reduce outdoor noise levels to 60 dB DNL or less for residential uses and 70 dB DNL for commercial uses such as offices. In those instances where the noise level is in excess of the standard, design practices shall be implemented to reduce noise levels in outdoor use areas.*

1. *Future residential or sensitive development along arterials and collectors shall not exceed County noise standards. Creative site planning shall be the primary means to achieve a 60 dB DNL noise level at the outdoor use area (i.e., side and rear yards, patios, balconies, decks, or enclosed courtyards). When necessary, building facades and noise barriers may be placed between the arterial roadway and the outdoor use areas (see Policy 6.44, “Edge Treatments for Use in Low- and Medium-Density Residential Area.”)*

2. *Commercial uses along Baseline Road and Watt Avenue and some residential uses along interior arterial streets, may be exposed to excessive noise levels. Where it is not possible to reduce noise in outdoor activity areas to 60 dB DNL or less in residential developments using a practical application of the best available noise reduction measures, an exterior noise level of up to 65 dB DNL may be allowed, provided that available noise level reductions have been implemented and interior noise levels are not in excess of 45 dB DNL. Acoustical analyses shall be prepared for all uses exposed to levels in excess of “normally acceptable” noise levels to show how both the outdoor uses areas and indoor noise thresholds shall be met in these locations.*
3. *All residences, hotels, motels, churches, nursing homes, and theaters uses exposed to a noise level in excess of 60 dB DNL will require sound-rated windows, added wall insulation, and mechanical ventilation capable of achieving the indoor noise requirements of 45 dB DNL, as determined by an acoustical analysis.*
4. *To determine compliance with noise standards, site specific acoustical analyses shall be required as a part of the Subsequent Conformity Review process, during the submittal of tentative subdivision designs and grading maps. Acoustical analyses shall be used to determine appropriate noise attenuation measures (i.e. setbacks, berms, building orientation, noise walls and other noise mitigation measures within the Placer County General Plan Noise Element and the design guidelines found under Section 6.4.3 “Walls, Fences, and Screening” of this Specific Plan) required to reduce traffic noise to levels that meet County noise level standards.*
5. *“The Landscape Master Plan” that will be subsequently prepared for the project shall include the design of noise attenuating features within the landscape setbacks and landscape corridor lots in the Plan Area, especially along Baseline Road, Watt Avenue, and Dyer Lane, where the greatest noise impacts are anticipated to occur.*

Policy 4.46 *Impacts of noise-generating uses will be minimized. Noise attenuation strategies shall be incorporated into all potential noise generating uses, and may include the following:*

1. *Outdoor use spaces shall be located behind buildings so that the building mass shields noise-sensitive uses from the noise sources.*
2. *Noise barriers shall be constructed between commercial uses and residences.*

3. *Limitations on hours of operation, maximum sound levels, and types of uses may be placed on the proposed uses of amplified sound at schools, parks, bars, restaurants, clubs, and other events.*
4. *Mechanical equipment noise at commercial and residential areas must be controlled. Methods may include quiet equipment, sound-attenuating enclosures, and noise barriers.*
5. *Full disclosure shall be required for all residential uses that are adjacent to, or directly across from schools, houses of worship, neighborhood parks, playgrounds, nightclubs, bars, and restaurants with live music and entertainment venues. The disclosure should state the typical hours of operation and noises associated with the use.*
6. *Additional acoustical analysis may be required for specific noise-generating activities that have the potential to adversely affect adjacent residences or other noise-sensitive uses (eg., hospitals, retirement homes, day-care centers, and schools) The analysis should identify the potential noise level and the means by which outdoor and indoor noise levels can be controlled to achieve the acceptable standards under the County Noise Ordinance.*

Policy 4.47 *Construction noise shall be controlled to meet applicable County codes and minimize annoyances on surrounding land uses. Construction noise abatement is critical in later phases of Placer Vineyards development when portions of residential neighborhoods are already in place. Mitigation measures to reduce constructing noise impacts may include the following:*

1. *Construction noise emanating from any construction activities for which a grading and building permit is required is prohibited on Sundays and federal holidays and shall only occur:*
 - a. *Monday through Friday, 6 a.m. to 8 p.m. (during Daylight Savings Time);*
 - b. *Monday through Friday, 7 a.m. to 8 p.m. (during Standard Time); and*
 - c. *Saturday, 8 a.m. to 6 p.m.*
2. *Truck traffic shall be routed through less noise-sensitive areas.*

CHAPTER V: TRANSPORTATION AND CIRCULATION



5.1 TRANSPORTATION/CIRCULATION CONCEPTS

Context

This chapter sets forth the policies for the transportation and circulation systems within the Placer Vineyards Specific Plan Area (Plan Area) with the goal of providing a variety of circulation choices in a safe and efficient manner. Placer Vineyards has been designed to accommodate a diverse range of transportation modes—roadways, street corridors, and open space trails—for automobiles, emergency vehicles, buses, and other transit services, and bikeways and paths that accommodate pedestrians, bikers, and equestrians.

Roadway layouts for the Plan Area and their cross section designs are also presented in this chapter. A summary of projected roadway volumes of streets affected by the project and their level of service conditions at major intersections is analyzed in Section 4.7, “Transportation,” in the Placer Vineyards EIR. The proposed intersection designs for Placer Vineyard’s roadways are provided in Appendix C of this document. Pedestrian and bike circulation policies and development standards for travel via on- and off-street trails and their corresponding cross sections are also contained in this chapter. For streetscape corridor and landscape design guidelines, refer to Chapter VI, “Community Design.”

5.2 EXISTING TRANSPORTATION CONTEXT

Regional Highways and Roadways

3 major regional highways and 2 main arterial roadways provide access into the Plan Area as indicated in Figure 5.1. Major regional highways include State Routes (SRs) 99 and 65 and Interstate 80 (I-80). The thoroughfare streets (identified as major arterial streets in the Placer County General Plan) include Base Line Road, which connects to Sutter County and the City of Roseville and Watt Avenue, which connects south to Sacramento County.

Located approximately 4 miles west of the Plan Area, SR 99, a 4-lane highway that runs north-south from the Central Valley (i.e., Chico and Bakersfield) into Sutter County. SR 99 is heavily traveled by trucks between these destinations.

4 miles east of the Plan Area is I-80, an 8-lane freeway that extends from the San Francisco Bay Area to Nevada and on to the East Coast. I-80 is the primary east-west freeway running through the Sacramento Valley region. SR 99 and I-80 both provide access south to downtown Sacramento, a regional employment center for area residents.

SR 65, generally a north-south road, is located about 5 miles northeast of the Plan Area and provides access from I-80 to SR 70. The highway begins as a 4-lane road in Rocklin and narrows to 2 lanes just north of Twelve Bridges Drive in Lincoln.

Base Line Road is a thoroughfare street running east-west on the northern border of the Plan Area connecting SR 99 and I-80. Base Line Road originates in Roseville as a commercial main street, becomes Riego Road as it crosses into Sutter County, and intersects with SR 99.

Watt Avenue is a thoroughfare street running north-south through the Plan Area. Watt Avenue terminates at Base Line Road on the northern border of the Plan Area, extends south into Sacramento County, and provides access to I-80.

Local Roadways

Local roadways providing access in and around the Plan Area include Fiddymment Road and Walerga Road.

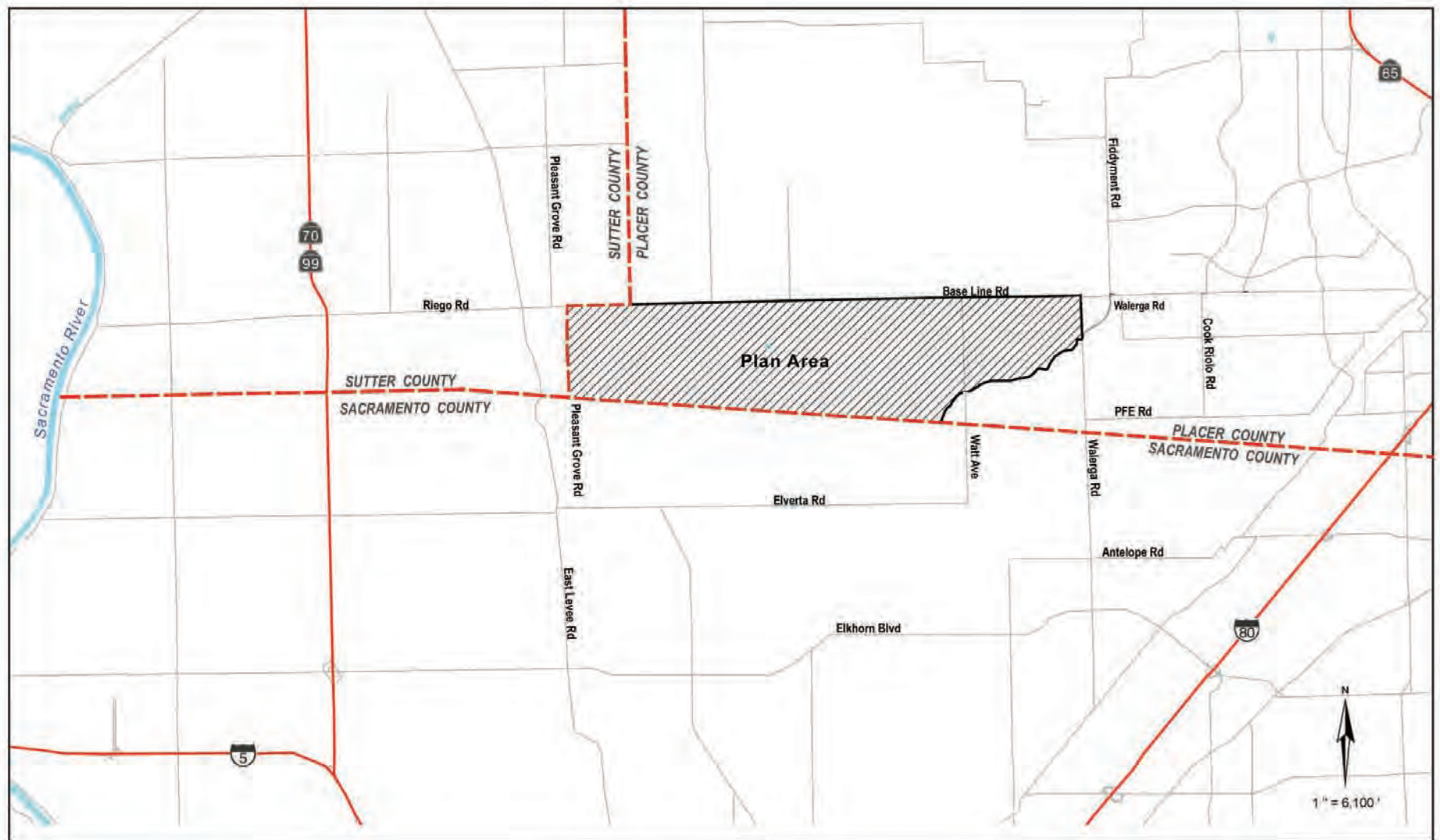
Fiddymment Road is a 2-lane undivided rural roadway that runs north from Base Line Road to Moore Road. It is the primary access road serving the surrounding residential developments.

Walerga Road is a 2-lane undivided rural roadway that extends from Roseville Road in Sacramento County and terminates at Base Line Road. Walerga Road provides access between the Antelope area and Roseville.

Several minor rural roadways also provide access to the Plan Area. Locust and Pleasant Grove Roads are 2-lane rural roadways of varying width and shoulder distances. They provide access within the Plan Area from Elverta Road to the south in Sacramento County up to Base Line Road. Palladay Road, Tanwood Road, Dyer Lane, and Colburn Street are minor rural roads that currently provide private access to properties within the Plan Area.



Figure 5.1 Local and Regional Roadways



5.3 TRANSPORTATION AND CIRCULATION ANALYSIS

Placer County's traffic model was used to determine the ability of the proposed circulation system to accommodate the anticipated traffic from the Plan Area. This model includes anticipated build-out of the Specific Plan plus development outside of the Plan Area.

Goal 5.1 Create and maintain a balanced, multi-modal transportation system that provides for the efficient and safe movement of people, goods, and services.

Policy 5.1 *Level of Service Standards.*

Within the boundaries of the Specific Plan Area and on its boundaries, the Placer Vineyards roadway system will be developed and managed to accommodate a Level of Service (LOS) D. Outside the Specific Plan Area, roadways shall conform to General Plan Standards that require the County to develop and maintain a minimum LOS "C" for rural and urban/suburban roadways, except within 1/2 mile of state highways where the standard shall be LOS "D," or as provided in Policy 5.2.

Policy 5.2 *Exceptions to General Plan Level of Service Standards. The County will allow exceptions to these LOS standards where it finds that the improvements or other measures required to achieve the LOS standards are unacceptable based on established criteria. In allowing any exception to the standards, the County shall consider the following factors:*

- 1. The number of hours per day that the intersection or roadway segment would operate at conditions worse than the standard*
- 2. The ability of the required improvement to significantly reduce peak hour delay and improve traffic operations*
- 3. The right-of-way needs and the physical impacts on the surrounding properties*
- 4. The visual aesthetics of the required improvement and its impact on community identity and characters*
- 5. Environmental impacts, including air quality and noise impacts*
- 6. Construction and right-of-way acquisition costs*
- 7. The impacts on general safety*
- 8. The impacts of the required construction phasing and traffic maintenance*

9. The impacts on quality of life as perceived by residents

10. Consideration of other environmental, social, or economic factors on which the County may base findings to allow an exceedance of the standards

Exceptions to the standards will be allowed only after all feasible measures and options are explored, including alternative forms of transportation.

5.3.1 PLAN AREA ROADWAY CIRCULATION

The new roadway circulation system for Placer Vineyards is based on a grid pattern of streets that organizes and provides access into the Plan Area. The Plan Area will be served by a network of public streets organized as a system of thoroughfares, arterials, major collectors, collectors, and local streets, as indicated in Figure 5.2.

Goal 5.2 Provide a complete network of transportation improvements including thoroughfares, arterials, collectors, and local roadways.

Goal 5.3 Locate roadways, wherever possible, adjacent to open space, public facilities, and multi-family residential and commercial uses to minimize the need for sound walls.

Goal 5.4 Minimize street widths, orient homes to front on low-volume collector streets, and provide landscape corridors that improve the streetscape environment.

Goal 5.5 Minimize traffic congestion in Placer Vineyards by discouraging regional thru-traffic on collector and local residential streets.

Policy 5.3 *Roadway System.*

The roadway system shall comply with Figure 5.2, the street sections in Figure 5.3, and the policies and design guidelines presented in this chapter and in Chapter VI, "Community Design." Figure 5.2 is intended to be a guide to internal roadway traffic needs. As each area is developed, additional roundabouts or traffic signals may be added in the future, as determined necessary by the County, for traffic flow and traffic calming. Local streets providing property access are not indicated in Figure 5.2.

Policy 5.4 *Street Section Design.*

The project shall dedicate rights-of-way of sufficient width to accommodate all future anticipated lanes, including auxiliary lanes, and intersection widening for dual left-turn lanes and free right-turn lanes. Refer also to Chapter VI, "Community Design," for more specific landscape and streetscape design guidelines and to Appendix C for traffic intersection designs.

1. *The landscape rights-of-way and parkways of all streets shall be dedicated at the same time as street rights-of-way.*
2. *The roadway measurements indicated in Figure 5.3 are measured to the back of the curb.*
3. *Thoroughfares: Base Line Road and Watt Avenue.*
The General Plan describes thoroughfares as major arterial streets designed to carry high volumes of thru-traffic with limited travel delay. Base Line Road and Watt Avenue shall be designed as thoroughfares with on-street bike lanes adjacent to 50-foot landscape corridors and meandering bike and pedestrian trails. They shall also include 20-foot-wide landscape medians. Drive-through breaks shall be provided at intervals along the roadway with medians at a spacing to be determined by the Department of Public Works in consultation with the California Department of Forestry.

Base Line Road is projected to become 6 lanes divided by a raised median. Figure 5.3, Section A, illustrates the proposed street sections for Base Line Road. At ultimate build-out, Watt Avenue is anticipated to be 6 to 8 lanes with 2 lanes dedicated for bus rapid transit (BRT) right-of-way. Figure 5.3, Section B, illustrates the proposed initial phasing and ultimate build-out street sections for Watt Avenue with and without the BRT system.

Thoroughfares will provide limited access at the locations indicated in Figure 5.2. No new connections shall be allowed on thoroughfare roads in addition to those shown in Figure 5.2, unless it can be shown that the new connection will benefit overall traffic flows. Access points shall be coordinated with the County to prevent driveways with parking along thoroughfare streets.

4. *Arterials: Dyer Lane and 16th Street.*
Arterial streets are high-volume streets with limited, controlled intersections. Their proposed street sections are illustrated in Figure 5.3, Sections D and E. Local and collector streets feed arterial streets to provide linkages between neighborhoods and major retail and employment centers. Arterial streets shall be designed to be 4-lane divided streets with 14-foot medians, on-street bike lanes, and 35-foot landscape corridors with a separated, multi-use trail. Drive-through breaks shall be provided at intervals along the roadway with medians at a spacing to be determined by the Department of Public Works in consultation with the California Department of Forestry.

Arterials will provide limited access with minimum intersection spacing at approximately every 1/4 mile (1,200 feet). Right-in and right-out access points without median breaks may be provided at a minimum spacing of

600 feet. To the extent possible, the street alignment for Dyer Lane shall be designed to avoid removing and/or disturbing the existing oak trees on Dyer Lane.

5. *Major Collector Streets: Palladay Road, Tanwood Avenue, and 14th Street.*
Major collector streets carry moderate traffic volumes. Major collector street sections are illustrated in Figure 5.3, Section F. Major collector streets provide access to individual development areas, neighborhoods, schools, parks, and other community amenities. Major collector streets are generally characterized as 2-lane roadways with on-street bike lanes, parallel parking, and separated tree lined sidewalks. The minimum distance from intersections to driveways shall be 600 feet or a distance determined appropriate by the County for safe access and traffic flow.
6. *Collector Streets*
Collector streets carry light to moderate traffic volumes. Collector street sections are illustrated in Figure 5.3, Sections G, H, TC1, and TC2. Collector streets provide access to individual development areas, neighborhoods, schools, parks, and other community amenities. Collector streets are generally characterized as 2-lane roadways with on-street bike lanes, parallel parking, and separated tree-lined sidewalks. For properties located adjacent to a neighborhood commercial development, the minimum distance from intersections to driveways shall be 300 feet or a distance determined appropriate by the County for safe access and traffic flow. The minimum distance from intersections to driveways may be less than 300 feet if determined appropriate by the County for safe access and traffic flow.
7. *Commercial Streets*
Commercial streets serve parcels within the commercial, business park, power center, and town center areas. They typically do not include bike lanes. Standards for commercial streets not included in the town center are shown in Figure 5.3, Section C. Standards and street sections for the town center commercial streets are provided in Figures 6.9 through 6.11 of Chapter VI, "Community Design."
8. *Local Streets*
Local streets are not located in the circulation diagram. They provide access to neighborhoods within the Plan Area and include non-residential and residential streets. Local streets are low traffic volume, 2-lane roadways with parallel parking, separated sidewalks, and tree-lined landscape parkways. Local streets will be determined in conjunction with specific site development at the time of tentative map submittal. Sections for local residential streets and cul-de-sacs are provided in Figure 5.3, Sections R1, R2, R3, R5, and R6.

9. Residential Alleys

Residential alleys, as shown in Figure 5.3, Section R4 shall be designed to the following standards:

- a. *Residential alleys are encouraged to be continuous through a block. However, if dead end alleys are allowed. Dead end alleys shall be no more than 150 feet long and shall provide visibility from one end of the alley to the other.*
- b. *Landscaping shall be provided, consistent with Policy 6.6-4.*

Policy 5.5 Private Local Streets.

Private local streets will be allowed within a development site subject to the review and approval of the County. Where access is to be provided by private local streets, the private street easement width can be less than 40 feet but cannot be less than the roadway dimensions (from back of curb to back of curb) shown on the street sections in Figure 5.3. Setbacks for private streets shall be measured from the back of the sidewalk. 12.5 feet wide public utilities easements (PUE) shall be provided adjacent to the private road for the benefit of utility companies serving the development. Exceptions to the 12.5 feet wide PUE may be granted upon approval by utility providers, if such easements are not needed.

Private streets shall be created as either a private roadway, public utility and emergency access easement, or as a separate parcel to be owned and maintained by a private entity, such as a Homeowner's Association. Placer County or a CSA will not accept the dedication or maintenance of private streets.

Policy 5.6 Locust Road Circulation Study.

Prior to improvement plans for the Core Backbone Infrastructure, as described in Section 9.3.1, herein, a study shall be undertaken by the County to identify and review the feasibility of alternatives to retaining Locust Road as a through roadway between Base Line Road and WestTown Center Drive. The study shall be funded by developers as provided in the Development Agreement. Any such study shall:

1. *Review the impacts upon the roadway systems in the Specific Plan and adjacent jurisdictions and identify the need for new or additional infrastructure, if any;*
2. *Examine the application of strategies contained in the County's Neighborhood Traffic Management Program and evaluate their effectiveness at addressing residents' concerns;*

3. *Include an analysis of the necessary amendments to the Specific Plan, the County General Plan, and/or the Dry Creek West Placer Community Plan to implement any such alternatives;*
4. *Identify the costs associated with any such alternatives; and*
5. *Specify compliance with the California Environmental Quality Act and any other applicable legal requirements.*

The County shall utilize the study to determine whether modifications to Locust Road are in the best interests of the County. The developers acknowledge that modifications to this Plan, the Plan Area roadway system, and required infrastructure, including the Backbone infrastructure, as the County may determine is necessary to effectuate the intent of this policy, may be required.

Roadway Design Guidelines

Roadways shall be designed according to the following guidelines:

1. Roads shall be designed for their dual roles as vehicular and non-vehicular transportation corridors with landscape berms or open space parkways, containing bicycle and pedestrian trails.
2. Local roadways shall be located to facilitate local circulation and shall discourage regional thru-traffic. Regional thru-traffic shall be concentrated on Base Line Road and Watt Avenue.
3. Thoroughfares, Base Line Road and Watt Avenue, shall be located at the perimeter of major development areas.
4. A finer grain network of connector streets shall be located to provide convenient access to all land use parcels.
5. East-west connector streets shall generally provide through connections between and through land use areas while north-south connector streets may be more discontinuous, terminating at parks, open space, and neighborhood entries.
6. Multiple points of access to development areas are encouraged, to maximize the number of streets that carry traffic and the distribution of traffic loads from each development area.



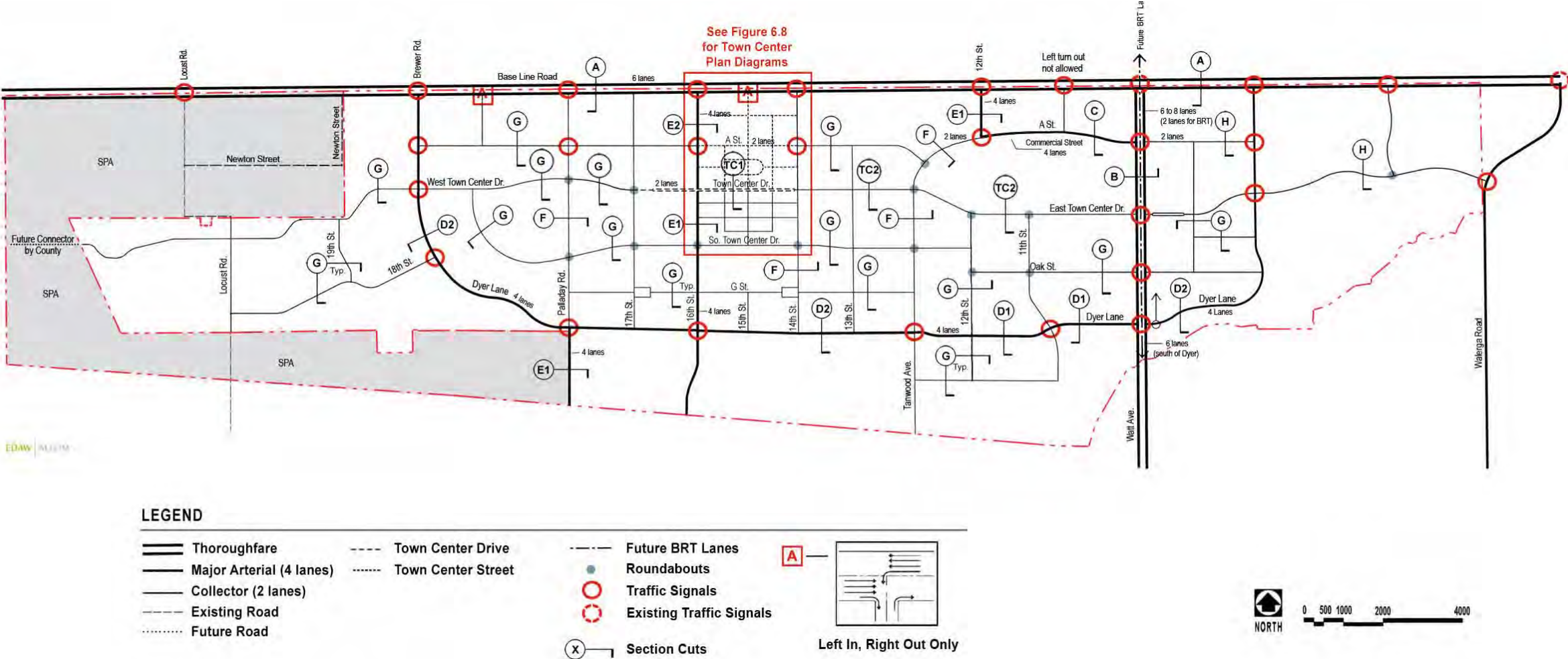
A street designed for both vehicular and non-vehicular transportation modes



A continuous arterial street designed to provide access to neighborhood development

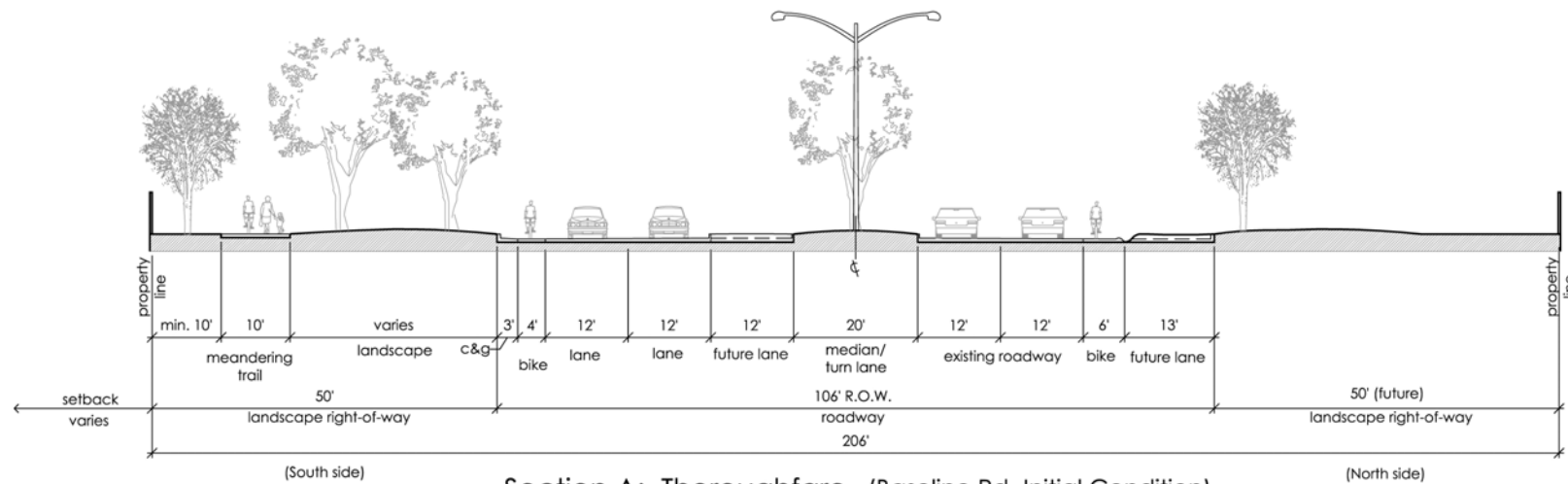
7. Neighborhoods should be designed with internal connecting streets to encourage a more open and accessible network for residents and improve the distribution of traffic throughout the roadway network. However, cul-de-sac roads are not excluded within residential areas as long as they are not excessively used.
8. Development areas and commercial sites shall be interconnected to allow for internal circulation and minimize impacts on adjacent arterial roadways.
9. Cul-de-sac roads should be no greater than 800 feet in length.
10. Streetscapes shall be designed in accordance with the design guidelines found in Chapter VI, "Community Design."

Figure 5.2 Roadway Circulation Diagram



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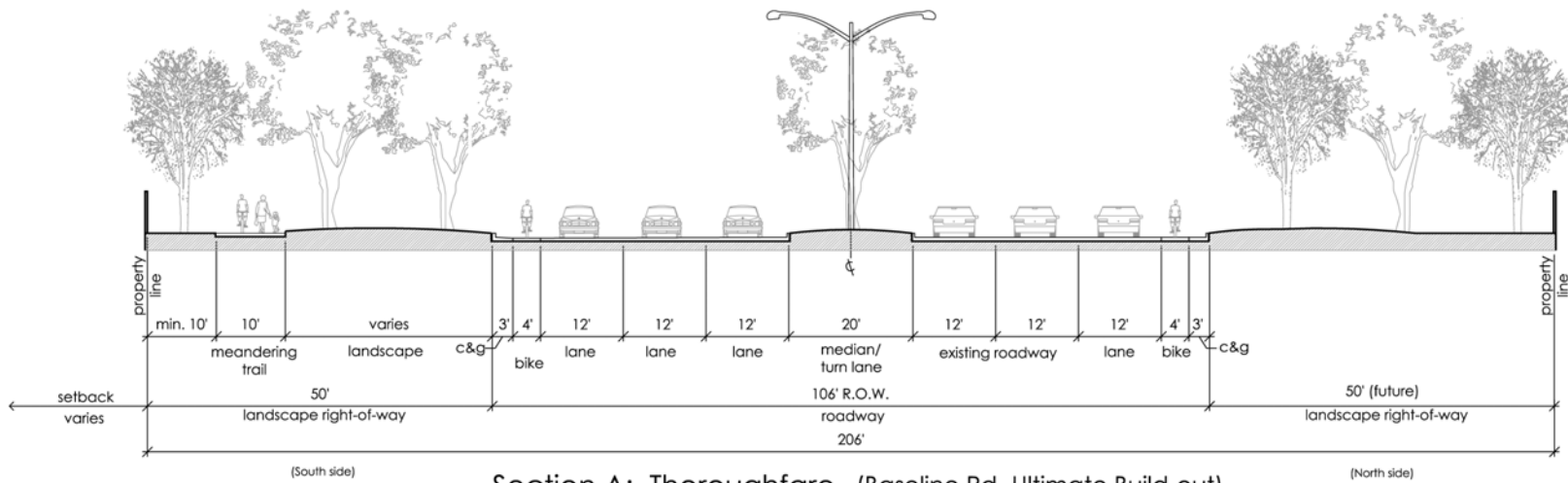
Figure 5.3 Roadway Sections—Thoroughfare (Base Line Road)

**LEGEND**

landscape	landscaping	bike	bike lane
lane	driving lane	c&g	curb and gutter
R.O.W.	right-of-way	min.	minimum

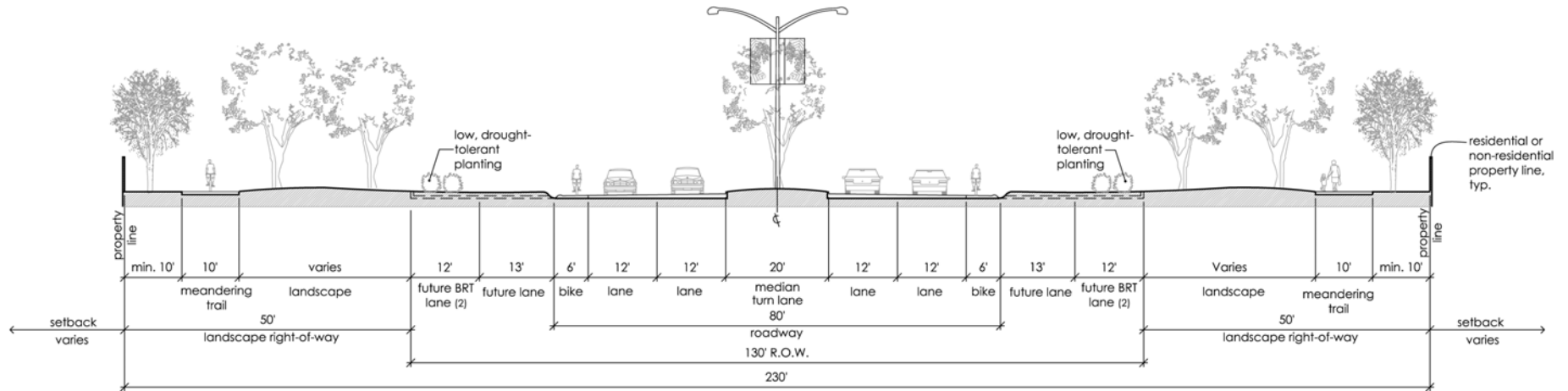
Notes:

(1) Core infrastructure system in initial phase of development

**Notes:**

(1) Plan Area build-out condition

Figure 5.3 Roadway Sections–Thoroughfare (Watt Avenue Phase 1 and 2)

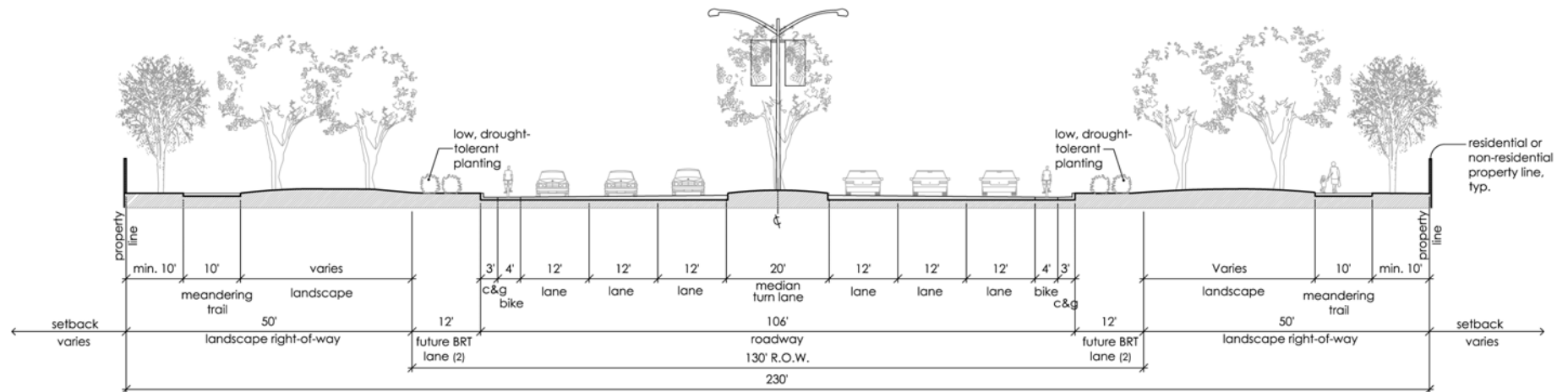


LEGEND

landscape	landscaping	bike	bike lane	c&g	curb and gutter
lane	driving lane	BRT	bus rapid transit		
R.O.W.	right-of-way	min.	minimum		

Notes:

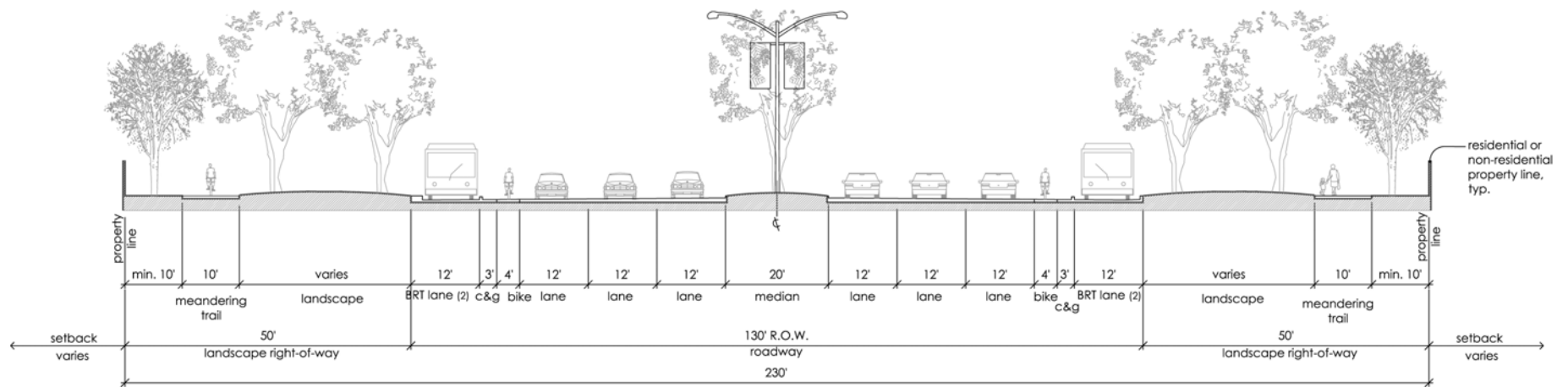
- (1) Core infrastructure in initial phase of development
- (2) Location of transit to be determined in the future



Notes:

- (1) Plan Area build-out condition with transit R.O.W. reserved
- (2) Location of transit to be determined in the future

Figure 5.3 Roadway Sections—Thoroughfare (Watt Avenue Phase 3)



Section B: Thoroughfare (Watt Ave.-Phase 3)

LEGEND

landscape	landscaping	bike	bike lane	c&g	curb and gutter
lane	driving lane	BRT	bus rapid transit		
R.O.W.	right-of-way	min.	minimum		

Notes:

- (1) Ultimate build-out condition with BRT transit
- (2) Location of transit to be determined in the future

Figure 5.3 Roadway Sections—Commercial Street

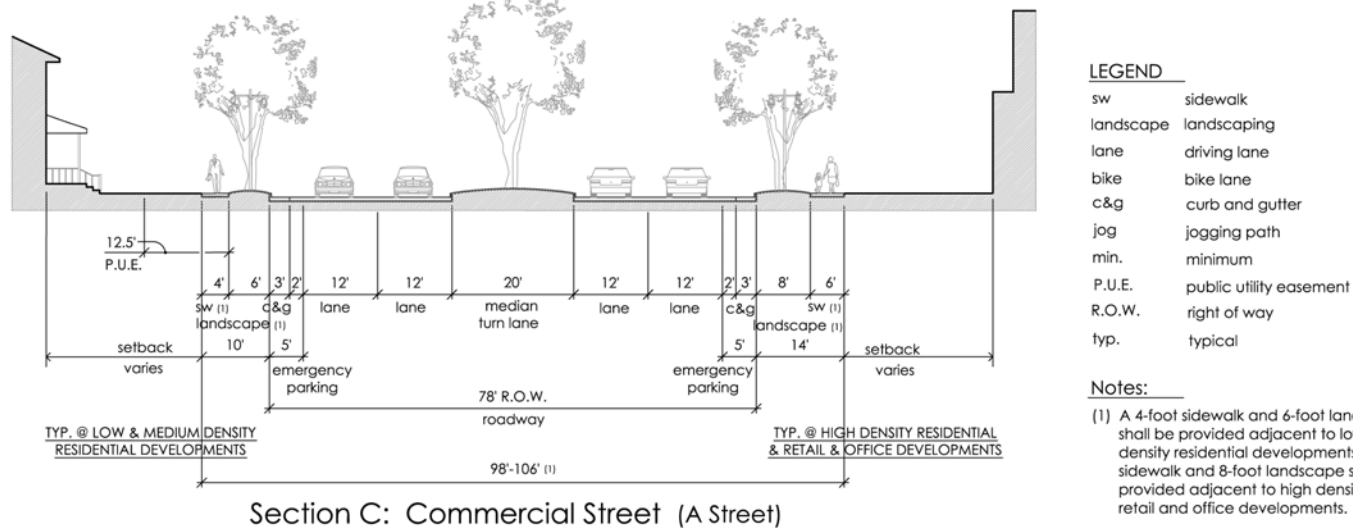
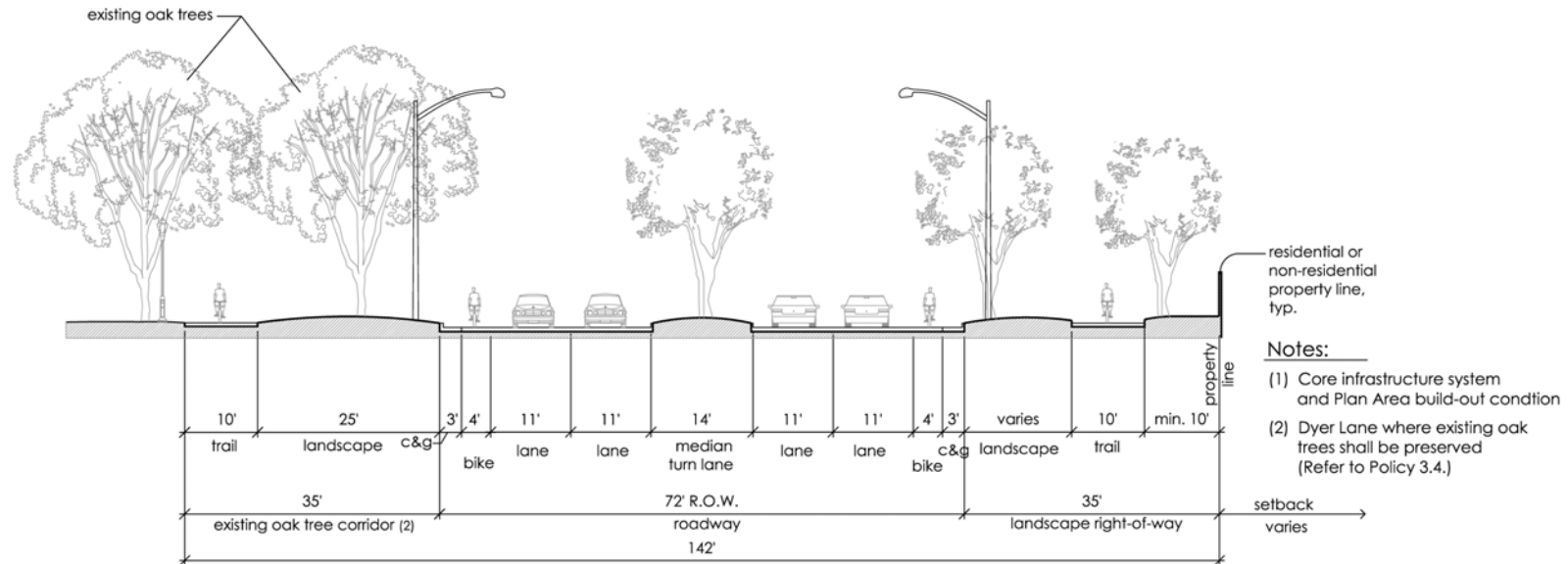
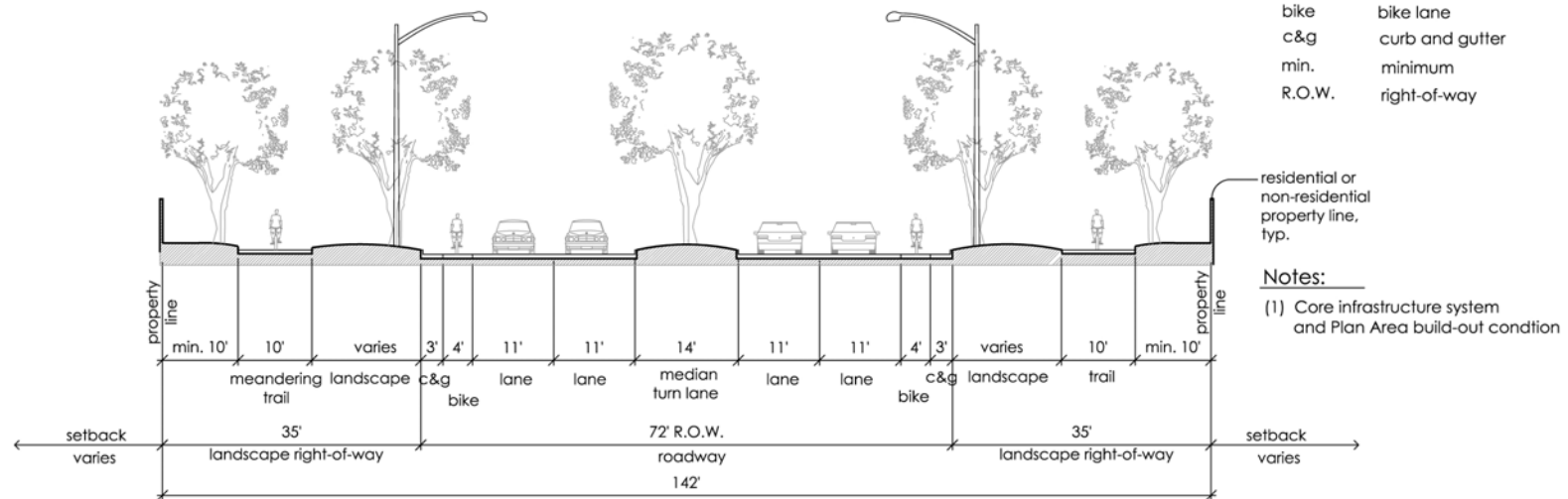


Figure 5.3 Roadway Sections–Major Arterial Street (Dyer Lane)



Section D1: Major Arterial Street (Dyer Lane-1)

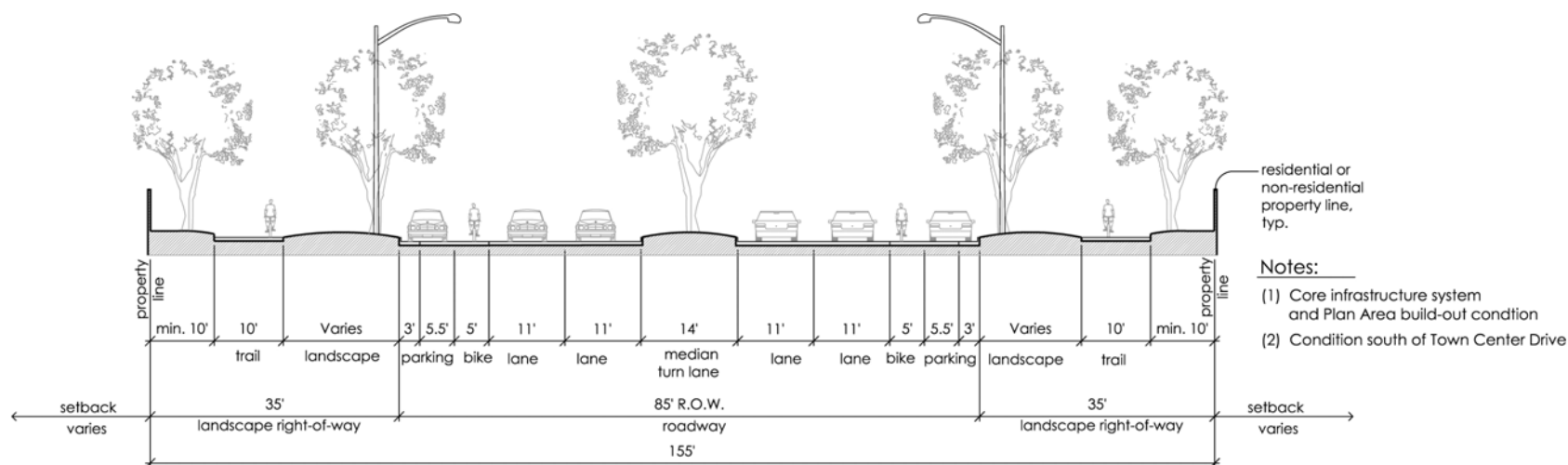


Section D2: Major Arterial Street (Dyer Lane-2)

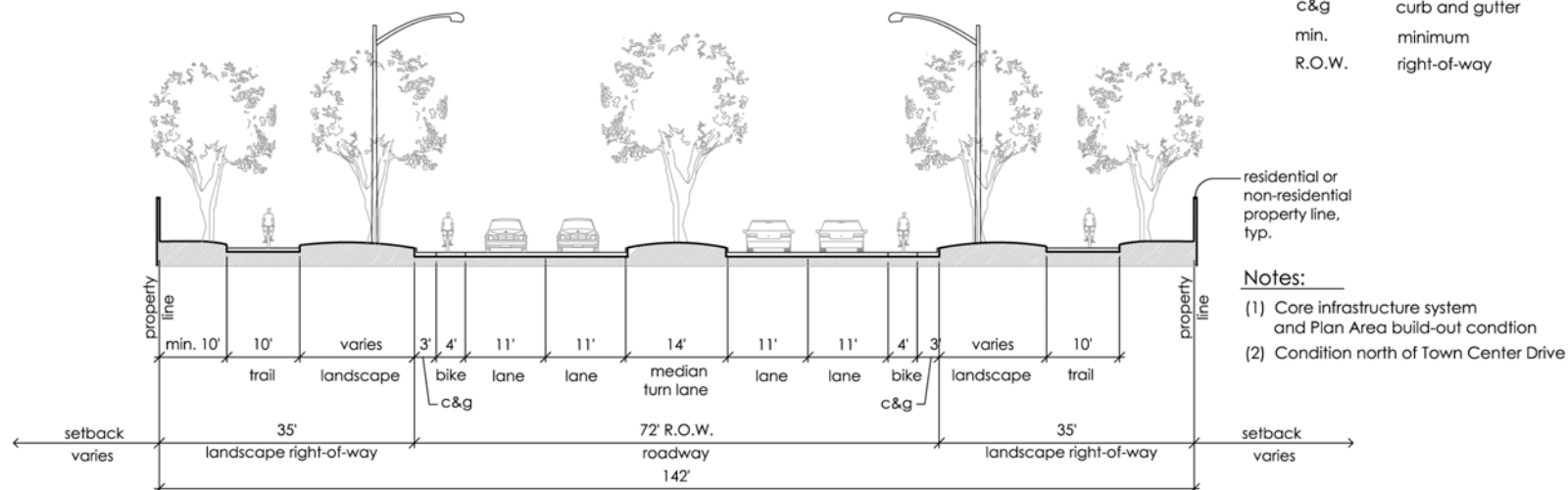
LEGEND

landscape	landscaping
lane	driving lane
bike	bike lane
c&g	curb and gutter
min.	minimum
R.O.W.	right-of-way

Figure 5.3 Roadway Sections–Major Arterial Street (16th Street)



Section E1: Major Arterial Streets (16th Street-South)



Section E2: Major Arterial Street (16th Street-North)

Figure 5.3 Roadway Sections–Major Collector Street

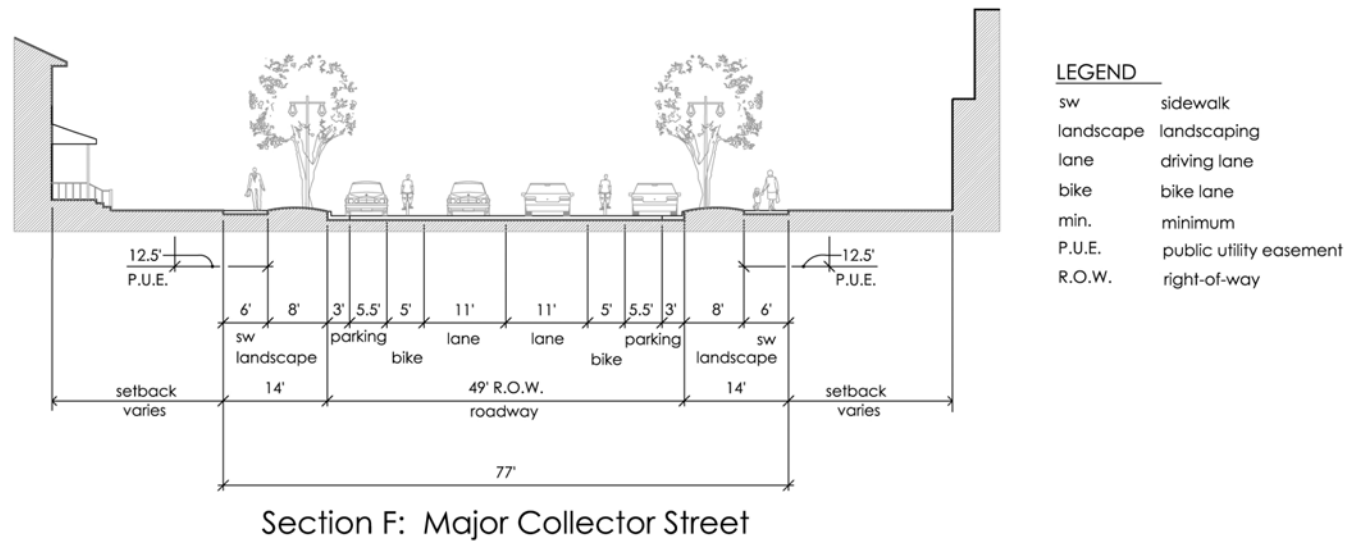
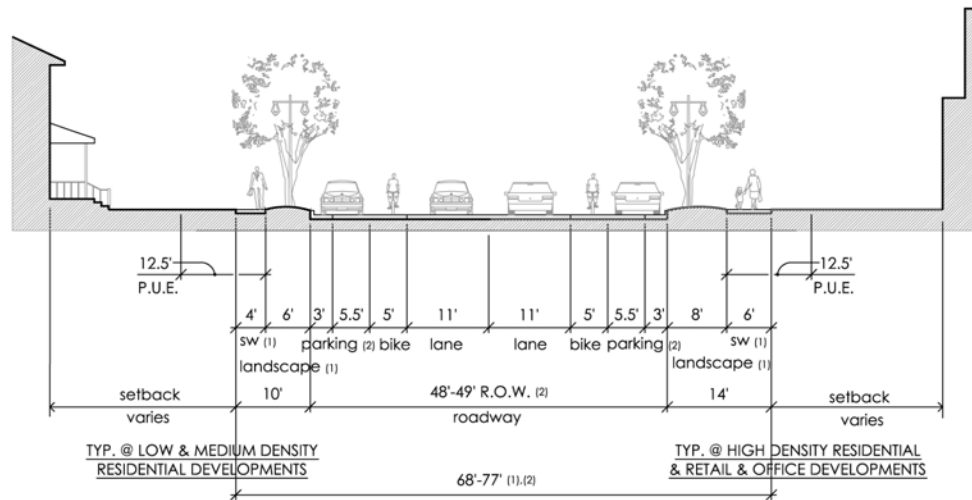
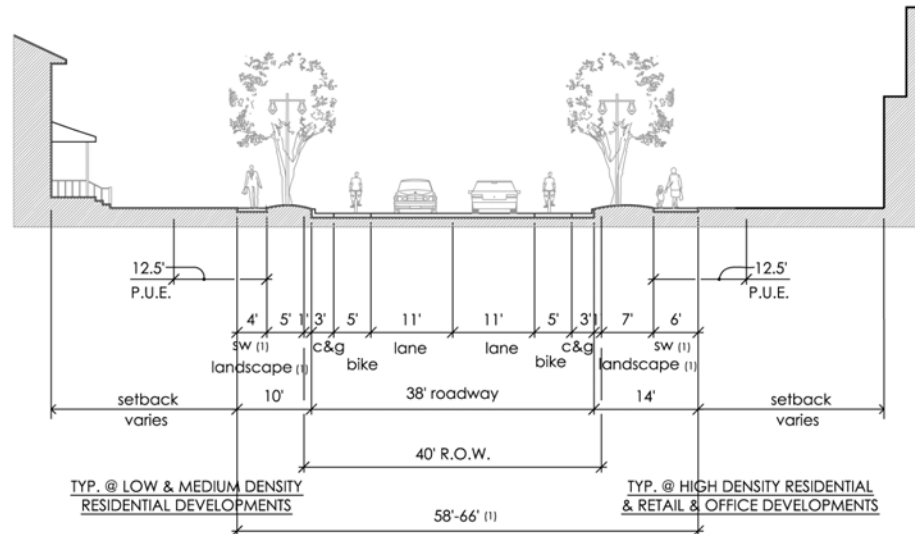


Figure 5.3 Roadway Sections—Collector Street



Section G: Collector Street



Section H: Collector Street

Notes:

- (1) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (2) Rolled curbs may be allowed subject to County review. Parallel parking lanes shall be 8-feet wide to back of curb at rolled curbs and shall be 8.5-feet wide to back of curb at vertical curbs.

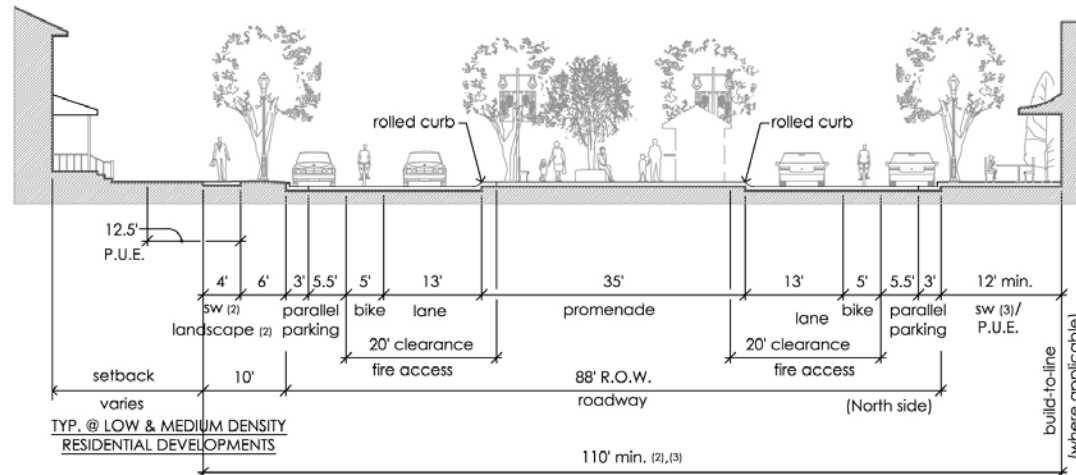
LEGEND

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
c&g	curb and gutter
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

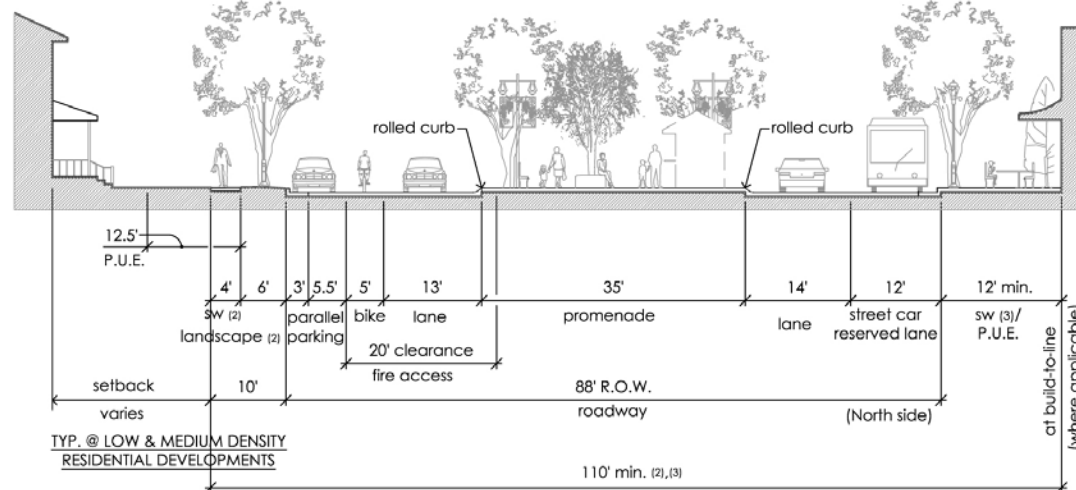
Notes:

- (1) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.

Figure 5.3 Roadway Sections–Town Center Drive



Section TC1: Town Center Drive (Initial Condition)

Section TC1: Town Center Drive (Build-out)
Elevation**Notes:**

- (1) Plan Area initial condition or condition without streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (3) The minimum overall sidewalk width in the Town Center shall be 12 feet with a preferred width of 16 feet. Sidewalks fronting retail and office developments shall maintain a minimum 6-foot wide clearance for pedestrian traffic. Also refer to Policy 6.29, "Town Center Sidewalks."

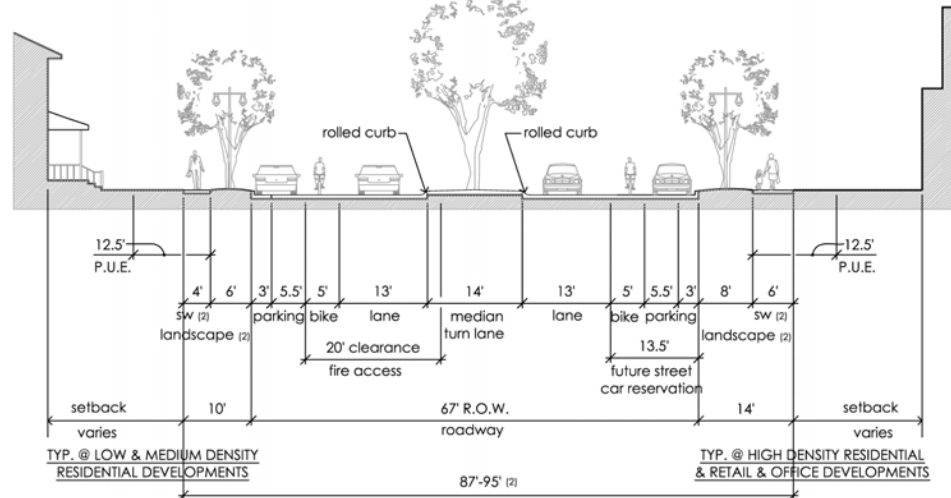
LEGEND

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

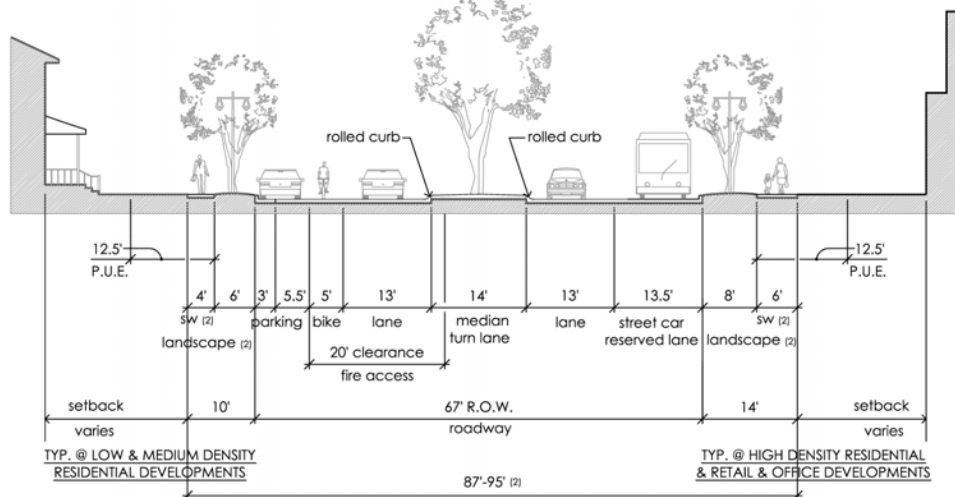
Notes:

- (1) Plan Area build out condition with streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (3) Sidewalks fronting retail and office developments shall maintain a minimum 6-foot wide clearance for pedestrian traffic. Also refer to Policy 6.29, "Town Center Sidewalks."

Figure 5.3 Roadway Sections–East Town Center Drive



Section TC2: Town Center Drive (Initial Condition)



Section TC2: Town Center Drive (Build-out)

Notes:

- (1) Plan Area initial condition or condition without streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.

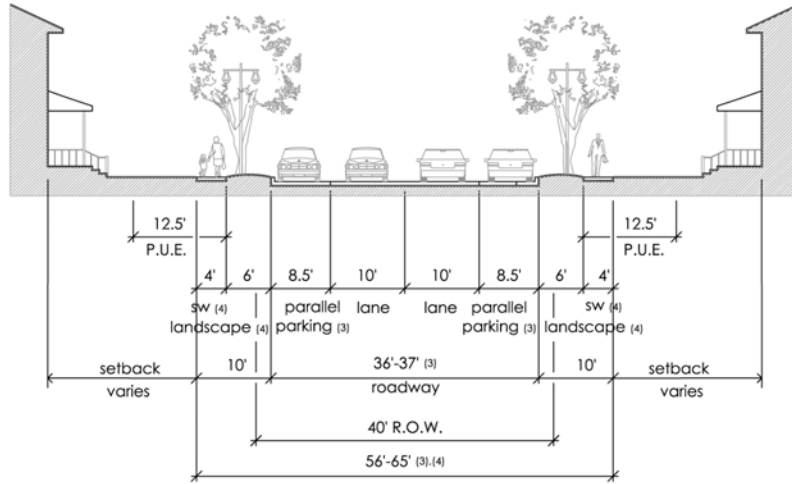
LEGEND

sw	sidewalk
landscape	landscaping
lane	driving lane
bike	bike lane
min.	minimum
P.U.E.	public utility easement
R.O.W.	right-of-way
typ.	typical

Notes:

- (1) Plan Area initial condition or condition with streetcar
- (2) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.

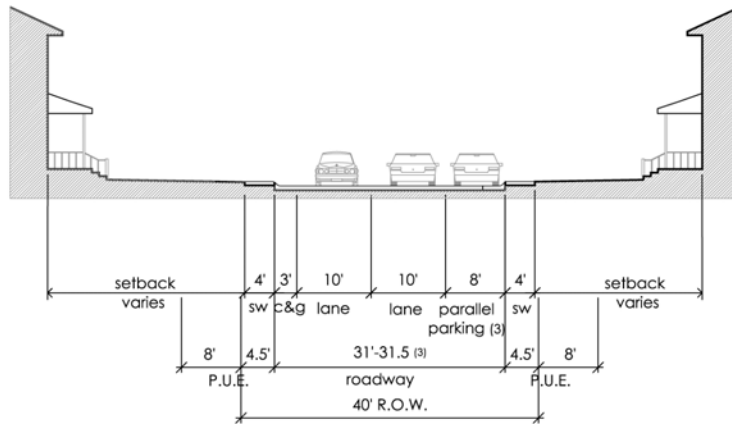
Figure 5.3 Roadway Sections—Residential Street



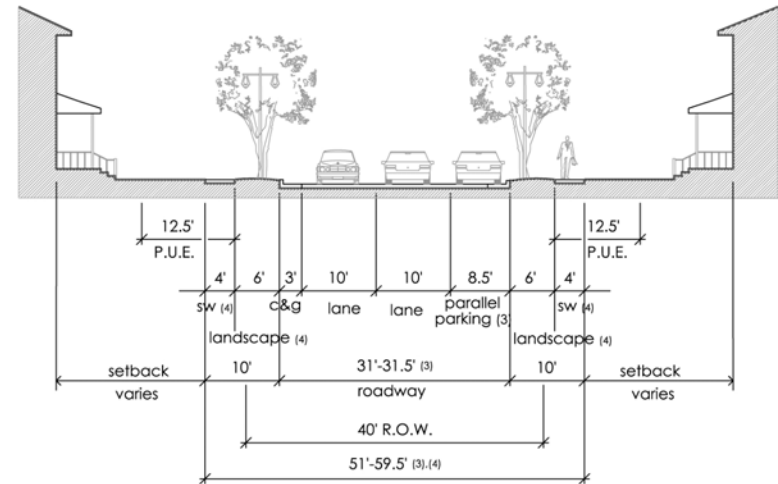
Section R1: Residential Street- Type A⁽⁵⁾
Parking on Both Sides

LEGEND

sw	sidewalk	bike	bike lane	c&g	curb and gutter
landscape	landscaping	P.U.E.	public utility easement		
lane	driving lane	R.O.W.	right-of-way		



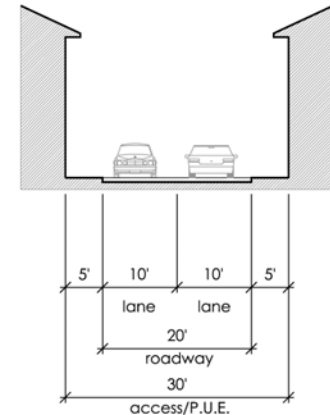
Section R3: Residential Cul-de-Sac Street⁽⁵⁾
(Ten Units or Less)



Section R2: Residential Street- Type B^{(2),(5)}
Parking on One Side

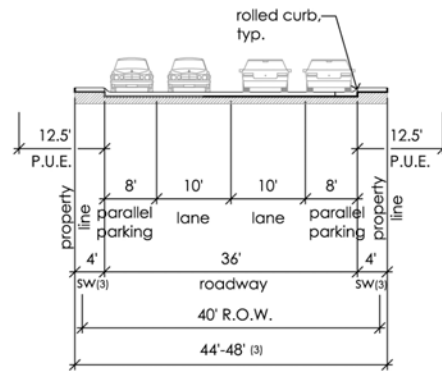
Notes:

- (1) A min. 12.5-foot P.U.E. is required adjacent to R.O.W. for all residential streets with exceptions provided upon approval by utility providers.
- (2) Parallel parking shall be provided only on one side of the street for single loaded street conditions, streets adjacent to open space and loop roads. (Refer to Section R2).
- (3) Rolled curbs may be allowed subject to County review. Parallel parking lanes shall be 8-feet wide to back of curb at rolled curbs and shall be 8.5-feet wide to back of curb at vertical curbs.
- (4) A 4-foot sidewalk and 6-foot landscape strip shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk and 8-foot landscape strip shall be provided adjacent to high density residential, retail and office developments.
- (5) Private residential streets may have a road easement width that is less than 40' but shall not have a roadway width (back of curb to back of curb) that is less than the roadway widths for residential sections R1-R4.



Section R4: Residential Alley

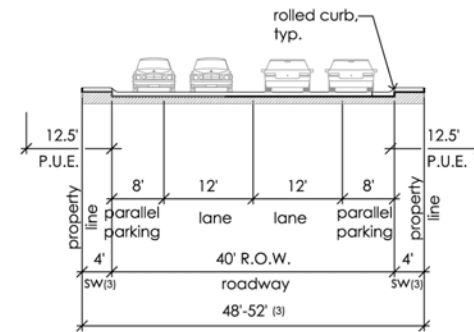
Figure 5.3 Roadway Sections—Residential Street



Section R5: Internal Residential Street
(In Age-Restricted Community)

LEGEND

sw	sidewalk	c&g	curb and gutter
landscape	landscaping	P.U.E.	public utility easement
lane	driving lane	R.O.W.	right-of-way



Section R6: Internal Residential Street
(In Age-Restricted Community)
(At High Volume Locations)

Notes:

- (1) A min. 12.5-foot P.U.E. is required adjacent to R.O.W. for all residential streets with exceptions provided upon approval by utility providers.
- (2) Parallel parking shall be provided only on one side of the street for single loaded street conditions, streets adjacent to open space and loop roads. (Refer to Section R2).
- (3) A 4-foot sidewalk shall be provided adjacent to low and medium density residential developments; a 6-foot sidewalk shall be provided adjacent to high density residential, retail and office developments.

5.3.2 TRANSPORTATION IMPROVEMENTS

Regional circulation improvements are identified by the Placer, Sacramento, and Sutter County General Plans and the *2004 Metropolitan Transportation Plan*, prepared by SACOG. Planned improvements include expansions to regional roadways, local roadways, regional transit systems, and trail systems needed to accommodate growth anticipated to occur in this region. These facilities will provide capacity to the surrounding local and regional developments. A number of local and off-site intersection improvements will be constructed as part of the Placer Vineyards development to accommodate cumulative traffic levels.

Policy 5.7 Regional Transportation Improvements.

Relative to the traffic impacts generated by the project, Placer Vineyards landowners and the County shall define Development Agreements to ensure that the project pays for its fair share of transportation improvements.

Policy 5.8 Off-site Transportation Improvements.

Placer Vineyards shall provide traffic signals and off-site intersection improvements, in conjunction with development in the Plan Area at the following locations:

1. Riego Road and East Natomas Road
2. Riego Road and Pleasant Grove Road
3. Base Line Road and Pleasant Grove Road
4. Watt Avenue and PFE Road

Policy 5.9 On-site Transportation Improvements.

The Placer Vineyards development shall fund and construct all transportation network improvements, including roadway design, traffic signalization, and traffic calming, necessary to support the new development when and as they are needed.

Policy 5.10 Concurrency.

Roadway improvements shall be constructed to coincide with the demands of new development as required to satisfy minimum level of service standards, as set by this Specific Plan.

Policy 5.11 Local Intersection Improvements.

Placer Vineyards shall provide local intersection improvements as guided by Figure 5.2.

1. *Signalized intersections for the thoroughfares, Base Line Road and Watt Avenue, are provided in Figure 5.2 at the following locations:*
 - a. *On Base Line Road, signalized intersections shall be located on Locust Road, Dyer Lane (west), Palladay Road, 16th Street, 14th Street, 12th Street, Tanwood Avenue, Watt Avenue, Dyer Lane (east), and Park Street.*
 - b. *On Watt Avenue, signalized intersections shall be provided at A Street, Town Center Drive (east), Oak Street, and Dyer Lane.*
 - c. *On Dyer Lane, signalized intersection shall be provided at A Street (east and west), Town Center Drive (east and west), 18th Street, Palladay Road, 16th Street, Tanwood Avenue, and 11th Street.*
 - d. *For commercial developments on A Street, signalized intersections shall be provided at Palladay Road, 16th Street, 14th Street, and 12th Street.*
2. *Base Line Road and Watt Avenue intersections shall be planned and designed to accommodate the needs of thru-traffic. This will include traffic synchronization and intersection designs that favor through movements and minimize conflict points. This may also include additional turning lanes or other special features, such as pedestrian amenities that highlight intersection crossings.*
3. *Roundabouts shall be located along the major east-west collector streets and lower volume traffic streets, focused at the intersections of residential neighborhoods.*
4. *Roundabouts shall be considered as an alternative, where all-way stops or traffic signals are indicated in the future (i.e., project build-out).*
5. *The County shall also reserve the right during the large lot or small lot tentative map process to require additional traffic signals or roundabouts, as determined to be necessary for traffic flow or safety.*
6. *The County shall also reserve the right to modify the minimum distance from a street intersection to a development driveway, as determined to be necessary for the traffic flow or safety of a specific site condition.*

Policy 5.12 Access within the Development Site.

Primary access to development will be avoided on high-volume arterial and thoroughfare roadways, and instead will be provided on collector or neighborhood streets and shall comply with the following standards:

- 1. Thoroughfares shall provide limited access. No driveways shall be permitted on Base Line Road or Watt Avenue. Access to properties fronting on Base Line Road shall be provided mainly from A Street and to a lesser extent from the roads that connect A Street to Base Line Road. Access to parcels from these connector roads shall be located at sufficient distance from Base Line Road so as not to impede the flow of traffic or create safety issues.*
- 2. Access to development sites from thoroughfares and arterials allowing for left turns into and out of the sites shall be limited to the identified signalized intersections in the Placer Vineyards Specific Plan, unless otherwise required under future development patterns.*
- 3. Minor right-turn-in and right-turn-out access points may be permitted by the County upon further detailed review and analysis of potential traffic and circulation impacts.*
- 4. See Figure 6.17 in Chapter 6, "Community Design," for conceptual site access designs for neighborhood commercial sites on high-volume roadways.*

Policy 5.13 Minimizing Barriers to Access.

The circulation and site plans for individual developments proposed within the Plan Area shall minimize barriers to access by pedestrians, the disabled, and bicyclists. Handicap ramps shall be incorporated into the design of all intersections and bicycle racks shall be located convenient to all retail, office, and civic sites.

Policy 5.14 Fire and Emergency Access.

All new development shall be coordinated with the local fire department to ensure that adequate emergency access is provided to all development areas and that emergency access routes are designed to the specification of the Placer County Fire Department.

Policy 5.15 Sound Walls.

Use of sound walls is discouraged. Where sound walls are required because of noise levels and traffic volumes on major streets, screen landscaping and mounding should be provided to minimize their visual impact and create a more attractive streetscape. Refer to Section 4.10 for additional policies related to noise levels and to Section 6.4.3 for wall, fence, and screening techniques and design guidelines.

5.3.3 TRAFFIC CALMING DESIGNS

Policy 5.16 Traffic Calming Roadway Design.




Use of traffic calming roadway design techniques in the design of residential streets and intersections is required. Techniques may include corner bulb-outs at intersections, traffic circles and rotaries, chokers, chicanes, etc. See the chart below and Figure 5.4 for recommended traffic calming designs. In all cases, traffic calming devices shall not restrict access by emergency vehicles or limit emergency response times below the required level of service standard.

A. Narrowing and Horizontal Devices




Traffic Calming Device	Description	Application
<p>Neckdown/Bulb-out</p>  <p>Source: ITE pedestrian bicycle council</p>	<p>Neckdowns/bulb-outs shorten the crossing distance of intersections and decrease the curb radii, reducing turning vehicle speeds.</p>	<p>Neckdowns/bulb-outs can be used at intersections or midblock locations on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>
<p>Two-lane choker</p>  <p>Source: ITE pedestrian bicycle council</p>	<p>2-lane chokers are midblock curb extensions that narrow a street. Chokers leave the street cross section with 2 lanes that are narrower than the normal cross section.</p>	<p>2-lane chokers should be used at midblock locations only on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>

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A. Narrowing and Horizontal Devices

Traffic Calming Device	Description	Application
<p>One-lane choker</p>  <p>Source: www.pedbikeimages.org/ City of Portland Office of Transportation</p>	<p>One-lane chokers narrow the roadway width so that there is only enough width to allow travel in one direction at a time. They operate similarly to one-lane bridges, where cars approaching on one side must wait until all traffic in the other direction has cleared before proceeding.</p>	<p>The example provided is the entryway into a residential development. One-lane chokers should be used at midblock locations only on lower order residential streets where the expected average daily traffic is less than 3,000 vehicles per day and the posted speed limit is 30 mph or less.</p>
<p>Center Island Narrowing</p>  <p>Source: www.pedbikeimages.org/ City of Portland Office of Transportation</p>	<p>These raised islands located along the centerline of a street narrow the travel lanes at that location. They can also be used at intersections to provide refuge for pedestrians when fitted with a gap for pedestrians to walk through.</p>	<p>Center island narrowings can be used on most roadway types where the expected average daily traffic is less than 20,000 vehicles per day and the posted speed limit is 35 mph or less.</p>
<p>Chicane</p>  <p>Source: www.pedbikeimages.org/ City of Portland Office of Transportation</p>	<p>These curb extensions alternate from one side of the street to the other, forming S-shaped curves. Chicanes can also be created by alternating on-street parking between one side of the road and the other.</p>	<p>Chicanes should be used at midblock locations only. Application where the expected average daily traffic is less than 5,000 vehicles per day and the posted speed limit is 35 mph or less is appropriate.</p>



A. Narrowing and Horizontal Devices

Traffic Calming Device	Description	Application
<p>Lateral Shift</p>  <p>Source: www.pedbikeimages.org/</p>	<p>These are curb extensions or other physical shifts of the roadway on otherwise straight streets that cause deflection.</p>	<p>Lateral shifts should be used at midblock locations only. Application where the expected average daily traffic is less than 5,000 vehicles per day and the posted speed limit is 35 mph or less is appropriate.</p>
<p>Traffic Circle</p>  <p>Source: www.pedbikeimages.org/</p>	<p>Traffic circles are raised islands, placed in intersections, around which traffic circulates. Stop signs or yield signs can be used as traffic controls at the approaches of the traffic circle. Circles prevent drivers from speeding through intersections by impeding the straight-through movement and forcing drivers to slow down to yield.</p>	<p>Traffic circles should be used at low volume residential intersections. The combined intersection volume should not exceed 10,000 vehicles per day.</p>
<p>Roundabout</p>  <p>Source: www.pedbikeimages.org/</p>	<p>Roundabouts are typically larger than neighborhood traffic circles and are used on higher volume streets to allocate right-of-way among competing movements. They have splitter islands to channel approaching traffic to the right, and do not have stop signs.</p>	<p>Single-lane roundabouts can be used at intersections where collector streets intersect. The combined intersection volume should not exceed 16,000 vehicles per day. A single lane roundabout may also be used in place of a traffic signal.</p> <p>Refer to Figure 5.4 for a typical roundabout design prototype for Placer Vineyards.</p>

B. Vertical Devices

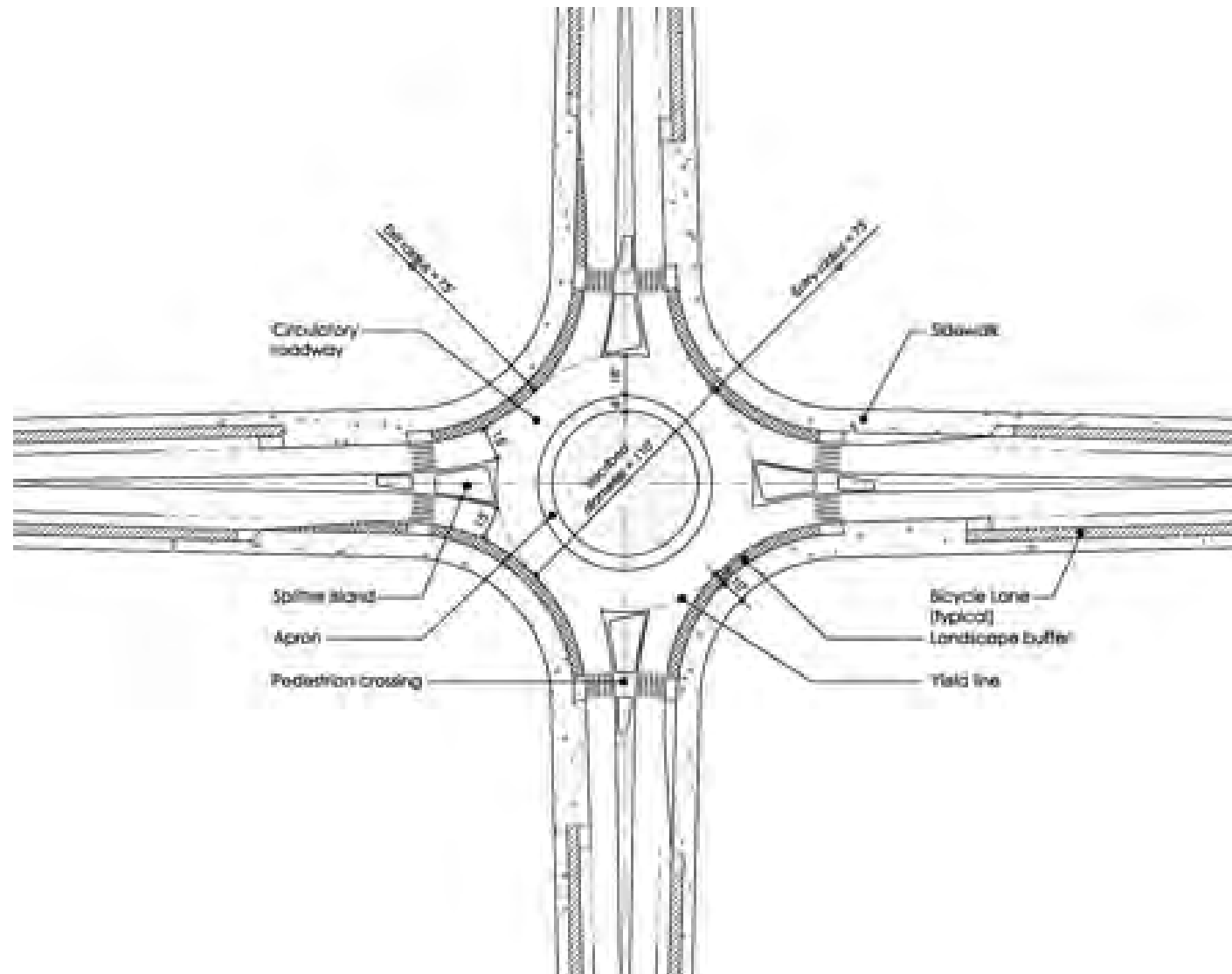
Traffic Calming Device	Description	Application
<div>Textured Pavement</div> <div></div> <div>Source: www.pedbikeimages.org/ Dan Burden</div>	<p>Textured colored pavement includes the use of stamped pavement (asphalt) or alternate paving materials to create an uneven surface for vehicles to traverse. It alerts drivers to a change in surroundings or emphasizes other traffic calming devices.</p>	<p>Textured pavement can be used at intersections, midblock locations, or driveways. This treatment can be applied to most roadway types, but should be limited in residential areas due to the noise created.</p>

C. Volume Control Devices

Traffic Calming Device	Description	Application
<p>Median Barrier</p> 	<p>These raised islands are located along the centerline of a street that continues through an intersection to block through movement at a cross street.</p>	<p>Median barriers are similar to center island narrowing but are used at intersection locations to prohibit certain turning movements. They can be used on most roadway types where the expected average daily traffic is less than 5,000 vehicles per day.</p>
<p>Forced Turn Island</p> 	<p>These raised islands prohibit certain movements on approaches to an intersection.</p>	<p>Forced turn islands channelize and/or restrict certain movements at an intersection. This treatment can be applied to most roadway types where the expected average daily traffic is less than 5,000 vehicles per day.</p>

Source: Fehr and Peers 2005

Figure 5.4 Typical Roundabout Design



5.3.4 TRANSIT SYSTEM

The Plan Area will include systems and facilities to promote public transit use, including dedicated bus rapid transit lanes on Watt Avenue from Base Line Road to the southern limits of the Plan Area, a transit center with bus turnouts, and provisions for future, local bus service on local roadways with bus turnouts and stops appropriately spaced.

An internal transit system shall be planned and implemented as the project is constructed. An Americans with Disabilities Act (ADA) dial-a-ride service will be provided. A fixed-route internal service will be provided that connects the village centers with the town center and other areas as deemed appropriate. Figure 5.5 shows the potential location of bus stops and the potential circulation routes for the public transit systems serving Placer Vineyards. Commuter service will be provided to downtown Sacramento. The transit center, proposed in the east village center along Watt Avenue, will serve as a major transfer point between regional and local transit service.

Goal 5.6 Promote public transit systems as an alternative means of transportation to reduce traffic congestion.

Policy 5.17 Bus Rapid Transit System.

A public transit system and dedication of right-of-way corridors for future bus rapid transit with a feeder bus network shall be provided along Watt Avenue from Base Line Road to the Dyer Lane intersection just north of Dry Creek.

Policy 5.18 Streetcar Right-of-Way.

Dedication of rights-of-way for a future streetcar system shall be provided along the north side of Town Center Drive, extending from the transit center on Watt Avenue to the town center, ending at 16th Street.

Policy 5.19 Multi-modal Transit Center.

A transit center will be located on Town Center Drive to serve as a transfer point for regional and local transit services. The transit center site shall be of sufficient size to accommodate all future anticipated uses. It will include covered shelters, bus staging areas, park-and-ride lots, and bicycle storage facilities.

Policy 5.20 Transit Service and Facilities

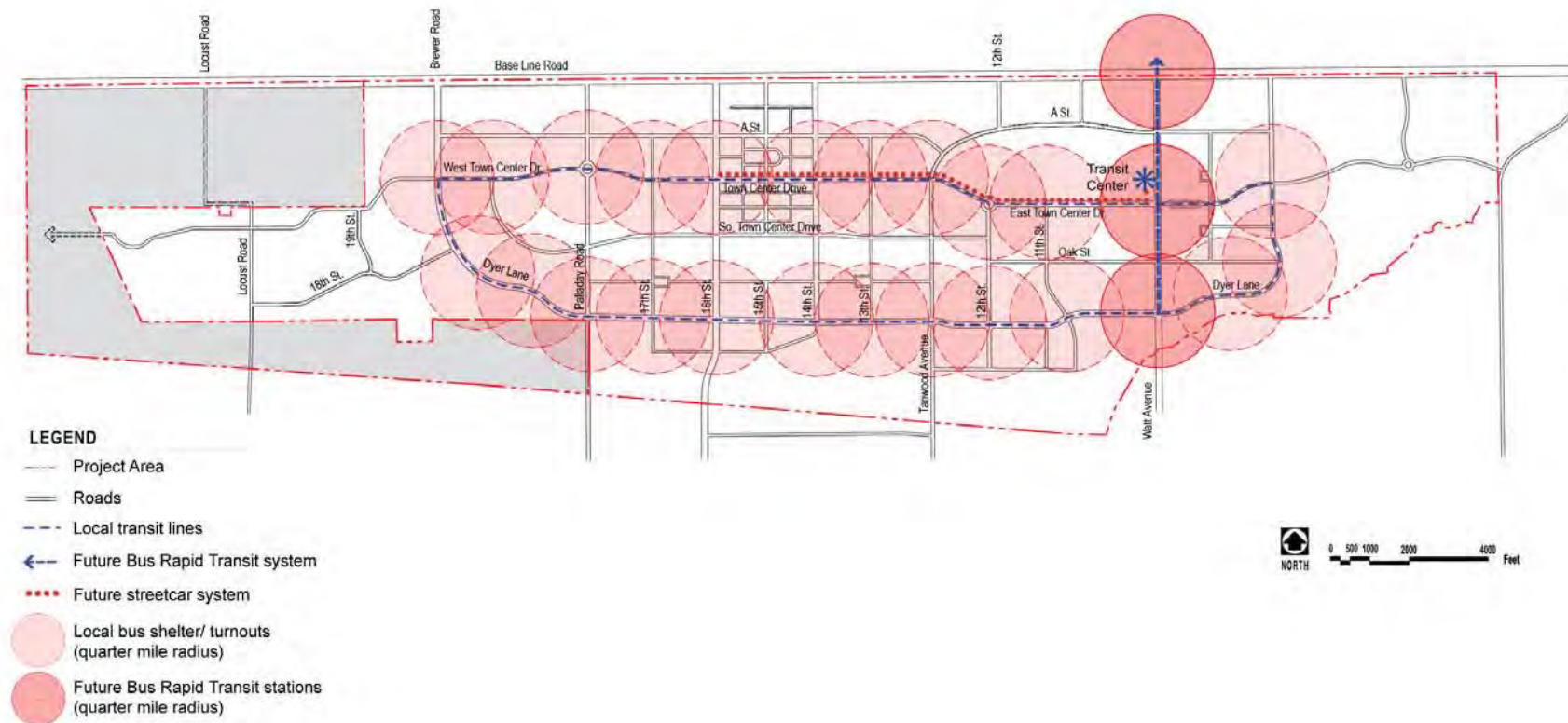
Placer Vineyards shall participate in regional service with connection to light rail transit on Watt Avenue in Sacramento County, Regional University, Galleria Mall, and other regional centers. As each parcel is developed, provisions for bus stops, turnouts, shelters, park-and-ride lots, bike lockers, lighting, and other transit-support facilities will be examined and constructed.

Policy 5.21 Provision of Park-and-Ride Lots.

Park-and-ride lots shall be established and maintained at the town center and transit center at the east village center. The majority of the park-and-ride spaces shall be accommodated in the transit center where a majority of local and regional commute trips will be concentrated. A minimum of 50 spaces shall be provided in the town center, established as shared parking. Other smaller park-and-ride lots are encouraged to be established as a shared parking use incorporated into the overall parking design of other commercial and office centers or adjacent to public transit.

In total, a minimum of 193 parking spaces shall be distributed between the park-and-ride lots. More park-and-ride lots should be provided, especially adjacent to neighborhood activity centers, transit routes, and major transit corridors to encourage ridesharing, promote use of public transit, and reduce air pollution.

Figure 5.5 Public Transit Concept



5.3.5 BIKE AND PEDESTRIAN CIRCULATION

There are 3 basic types of bikeway trails as defined by the *Placer County Bikeway/Trails Master Plan*. They are:

Class I Bicycle Trails:

Off-street pathways completely separated from traveled roadways for the exclusive use of bicycles and pedestrians

Class II Bicycle Lanes:

Signed and delineated on-street lanes designed for one-way use of bicycles. Class II lanes are typically located along the shoulder or gutter in a widened portion of the street.

Class III Bicycle Routes:

Non-designated, on-street routes along local public streets where bicyclists do not have a delineated lane and must share the roadway with motorists.

Class I Bicycle Trails:

The proposed off-street trail system for Placer Vineyards provides more than 46 miles of Class I bike trails, located within open space and landscape corridors along thoroughfares and arterial streets. Class I bike trails shall generally be 10-foot-wide hard surface paving, except along the Dry Creek corridor. Trails along the Dry Creek corridor shall be 12 feet with a 2-foot-wide decomposed granite jogging path on one side of the hard surface paving. The location of Class I bike trails are indicated in Figure 5.6, "Off-Street Trails Diagram," and the roadway sections in Figure 5.3.

Off-site trail dedications may be requested in association with tentative subdivision maps if it is determined by County staff that such linkages are crucial in the ability to complete major trail segments.



Class I Bicycle Trails



Class II Bicycle Lane



Class III Bicycle Route

Class II Bicycle Lanes:

Class II bicycle lanes will be located within the right-of-way of arterial, major collector, and collector streets. Bike lanes shall be designated with a white, painted stripe on the roadway. Street signs shall indicate the location of bike lanes and major destination points.

Class III Bicycle Routes:

Class III bicycle routes will be located on existing traffic lanes with low traffic volumes. These streets will connect to Class II bike lanes and Class I bike trails.

Goal 5.7 Provide a system of on- and off-street trails that connect to destinations within the Plan Area and to the regional trail network.

Policy 5.22 Trail System.

Trails shall be provided as identified by Figure 5.6, "Off-Street Trails Diagram."

Policy 5.23 Types of Trails.

Trails shall be provided within the Plan Area that offer a variety of experiences, including trails within and between parks and other public open space lands or to schools, and trails that connect to regional trails and transit facilities within and outside of the Plan Area.

Policy 5.24 Provision of Trails.

Private developers shall incorporate trail routes that are within their proposed tentative maps as identified in the trails diagram (see Figure 5.6). Placer Vineyards trails shall conform to the following standards:

- 1. In the Dry Creek corridor only, Class I bicycle trails shall be 12-foot wide hard surface paving with a 2-foot-wide decomposed granite path on one side of the paving.*
- 2. In all other areas, Class I bicycle trails shall be 10-foot wide hard surface paving.*

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3. *In open space areas, natural surface (gravel, earth) multi-use trails may be set a minimum of 10 feet off the hard surface paved trail (for activities such as equestrian riding and mountain biking).*
4. *Informational signs will be placed throughout the trail system (e.g., “2.4 miles to town center”).*
5. *Trails will be set back a minimum of 10 feet from residences.*
6. *Trails will be set back a minimum of 25 feet from preserved or reconstructed wetlands, whenever possible.*
7. *Collapsible bollards or other similar devices approved by the County will be placed at entries to restrict vehicular access where trails and streets intersect.*
8. *Trail crossings of drainage ways will occur at appropriate intervals.*
9. *Traffic calming methods and signage shall be used to enhance the safety of the trail systems where they cross major or collector streets.*
10. *A Class I trail crossing shall be provided under the Watt Avenue bridge within the Dry Creek corridor.*
11. *A Class I trail shall also be provided on the east side of the Dry Creek bridge and along Watt Avenue, extending to the Placer/Sacramento County line. The Class I trail on the east side of the bridge will be separated from traffic by a railing.*

Policy 5.25 Construction of Bike Trail Improvements.

Bike trail improvements are planned to connect Morgan Creek to Gibson Ranch Park. Landowners shall design and construct bike trail improvements within the open space portions of their property, according to the following standards and provisions of the Development Agreement.

1. *In conjunction with the construction of a core backbone roadway system, a set of core backbone trails adjacent to these roadways, as described in Section 9.3 and the Public Facilities Financing Plan, shall be constructed at the same time that the core backbone roadways are constructed.*
2. *Landowners shall install sections of the trail when they install subdivision improvements within the parcels adjacent to the open space. Trail connections to the core backbone trails shall be included as part of the subdivision improvements.*

3. *Bike trail sections shall be constructed and improved according to Figure 5.6, “Off-Street Trails Diagram.” Bike trails shall be designed in accordance with the County’s design standards for off-street bike trails and the guidelines provided in the Specific Plan.*
4. *Landowners shall proceed to complete the construction of bike trail improvements at the same time that they install and complete the balance of the subdivision improvements for the parcel(s) adjacent to the open space.*
5. *Landowners shall be responsible for all costs associated with the design and construction of bike trail improvements, including the costs of preparing required plans and drawings and obtaining all required permits.*
6. *Upon completion of bike trail improvements by the landowner, the County shall accept the dedication of the bike trail and applicable open space area and assume ownership and maintenance of these facilities, provided that the cost of maintenance shall be funded by the CSA.*
7. *Construction of a bicycle/pedestrian crossing over or under Base Line Road shall be coordinated with development in the north with the City of Roseville, in conjunction with the Sierra Vista plan.*

Policy 5.26 Fire Trails/Access through Open Space.

Fire access routes shall be integrated into the open space trails system and shall comply with Placer County Fire Department standards.

1. *Open space trails shall include design features that minimize barriers to emergency response, such as knock-down bollards for emergency access at trailheads.*
2. *Rolled curb access points shall be provided in open space areas. Open space access points shall be provided at each cul-de-sac that abuts an open space and spaced every 1,000 feet along streets adjacent to open space areas. These access points shall be identified with signage and painted red curbs. Emergency access easements shall be provided for each emergency access area.*

Policy 5.27 Roadway Crossings in Utility Corridors.

Roadway crossings shall be minimized through utility corridors to reduce the fragmentation of trails and open space.

5.3.6 EQUESTRIAN CIRCULATION

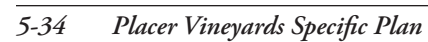
Policy 5.28 Provision for Equestrian Trails.

Equestrian trails are located in the open space buffer areas adjacent to the SPA and next to the Placer/Sacramento County line, as indicated in Figure 5.6, “Off-Street Trails Diagram.” The equestrian trails planned adjacent to the SPA will be separated from the Placer Vineyards development area with landscape berms (refer to Figures 7.10-7.14 for plans and sections of the berms and buffers next to the SPA). The equestrian trails planned in the open space buffer areas along the Placer/Sacramento County line, shall connect to the facilities at Gibson Ranch Park and link into the equestrian trail system proposed along the south side of Dry Creek, as directed by the Dry Creek/ West Placer Community Plan.

Policy 5.29 Equestrian Trail Connections between the SPA and the Urban Plan Area.

Future development in the SPA properties located south of Dyer Lane (between Palladay Road and El Verano Avenue, on parcel numbers 023-010-007, 023-190-007, 023-190-027, 023-190-028, 023-190-029, and 023-190-030) where a gap in the equestrian trail occurs, shall be required to connect to the equestrian trails in Placer Vineyards. The construction of equestrian trails will be required to be completed at the same time that subdivision improvements are completed for the applicable SPA parcel.

Figure 5.6 Off-Street Trails Diagram



CHAPTER VI: COMMUNITY DESIGN



6.1 COMMUNITY DESIGN CONCEPT

Context

This chapter is intended to define the type of community envisioned for Placer Vineyards. This chapter sets the goals, policies, and design guidelines that ensure the creation of a safe, high quality, and livable environment for the existing and new residents of Placer Vineyards. The “Community Design” chapter along with those standards and guidelines in other chapters of the Specific Plan are intended to ensure a cohesive and unique character for Placer Vineyards, while allowing flexibility for individual property owners to develop their own unique brand of housing types and distinct neighborhoods.

The contents of this chapter shall be used in concert with the standards and guidelines found elsewhere in the Specific Plan. Refer also to the following chapters of the Specific Plan for information regarding the design of the Placer Vineyards community:

- ♦ Chapter III, “Land Use,” introduces the land use plan and describes the land uses in the community;
- ♦ Chapter V, “Transportation and Circulation,” provides the design standards and guidelines related to the design of the streets and trails within the Placer Vineyards site;
- ♦ Chapter VII, “Parks and Open Space,” describes the parks and open space network for Placer Vineyards;
- ♦ Chapter VIII, “Public Utilities and Services,” provides information on the design of the infrastructure system serving the Plan Area as well as the schools, sheriff, fire protection, government services and other community serving facilities;
- ♦ Chapter IX, “Implementation,” discusses the administration and implementation steps required to achieve the goals and regulations set forth in this Specific Plan;
- ♦ Appendix A, “Land Use and Development Standards,” further defines the intensity and density of development and permitted uses allowed in each of the land use districts; and
- ♦ Appendix B, “Recommended Plant List,” provides a list of the recommended trees, shrubs, vines, and ground covers for Placer Vineyards.

Purpose

In general, the standards and guidelines of the Specific Plan are intended to be consistent with adopted County goals and policies. This chapter of the Specific Plan establishes specific standards for the Plan Area that may be unique or different from the *Placer County Design Guidelines* and *Placer County Landscape Design Guidelines*. This chapter addresses the overall vision for Placer Vineyards and will serve to implement and direct development specific to the Placer Vineyards site.

Application of these standards and guidelines ensures that development that may occur over an extended period of time is high quality. The Placer Vineyards development will result in distinct neighborhoods and development areas with unique site and landscape characteristics. Different areas of the site will be united by a common network of streets, trails, and greenways; connected by common access to an extensive system of parks, open space, schools, employment, public facilities, and urban centers; and united by a townscape vocabulary achieved through the consistent treatment of common site and building design elements.



COMMUNITY DESIGN

The standards and guidelines in this chapter address the common design elements, the centers, and unique residential neighborhoods in Placer Vineyards. It is organized into the following sections:

6.2 Community-wide Design Elements

- 6.2.1 Landscape Design
- 6.2.2 Streetscapes
- 6.2.3 Community Gateways
- 6.2.4 Signage Systems
- 6.2.5 Lighting

6.3 Centers of Activity

- 6.3.1 Design of Activity Centers
- 6.3.2 Town Center
- 6.3.3 Village Centers
- 6.3.4 Commercial Centers
- 6.3.5 Base Line Road Regional Commercial Corridor
- 6.3.6 Neighborhood Commercial Centers

6.4 Residential Neighborhoods

- 6.4.1 General Lot Design Standards
- 6.4.2 Residential Design
- 6.4.3 Walls, Fences and Screening

The standards and guidelines in this chapter will be used to assist Planning staff, the Planning Commission, and Board of Supervisors in evaluating the merit of development proposals. To aid in the interpretation of policies and guidelines in this chapter, the following rules apply:

- ♦ Policy statements and design standards express a County requirement and use the terms, “shall” or “must.”
- ♦ Design guidelines are qualitative and express a design intent through descriptions and illustrations, with flexibility for interpretation so long as the intent of the guidelines is upheld. Guideline statements use the words “should,” “may,” “encouraged,” and “discouraged.”

- ♦ Guidelines that employ the word “should” are intended to express the County’s desire and expectation. An alternative measure or approach may be considered, however, if it meets or exceeds the intent of the design guideline. Guidelines that include “may” allow for flexibility and expresses that alternative measures or approaches can be used to satisfy the intent.
- ♦ Guideline statements using the words “encouraged” or “discouraged” are meant to express, respectively, a more or less desirable design solution.

Should individual projects differ in some respects but stay substantially consistent with the intent of these design standards, minor deviations may be allowed. These determinations will be made at the discretion of the Planning Director or the Planning Commission. For design items that are specified in exhibits, text, and/or adopted goals and policies of the Specific Plan, modifications may require an amendment to the Specific Plan. Minor changes to an approved project that are consistent with original approvals may be approved by the Planning Director. Refer to Chapter IX, “Implementation,” for the administrative procedures to follow.

6.2 COMMUNITY-WIDE DESIGN ELEMENTS

This section outlines the goals and policies applicable to plan elements that are to be consistent across the entire Plan Area. For certain elements (i.e. parks, open space, streets) the text may also refer to design standards contained in other associated chapters of this Specific Plan.

6.2.1 LANDSCAPE DESIGN

The designed landscape creates the framework for common elements that knit the community together. These elements are intended to bring cohesiveness to the community, to engage the natural vegetation and topography of the site, to enhance the livability and identity of the community, and to provide transitions to adjacent sites and land uses.

- Goal 6.1** Provide a consistent and visually cohesive landscape throughout the Plan Area as well as provide identity to individual neighborhoods.
- Goal 6.2** Define and maintain a landscape and architectural style for the Plan Area.
- Goal 6.3** Provide for views from the road to community features such as creeks, wetlands, major tree groves, and other open space on the site.

Goal 6.4 Put in place a designed landscape that utilizes a plant palette that is visually attractive, varied and either uses recycled or reclaimed water or minimizes water use.

Policy 6.1 Landscape Design and Landscape Master Plans.

In order to ensure the implementation of a cohesive image for the community, a detailed Landscape Master Plan shall be developed and approved by the County. This Master Plan shall address the design of streetscapes, entries, and other image features. The Landscape Master Plan and the Park and Recreation Master Plan (see Chapter 7, "Parks and Open Space") shall complement each other and together shall define the public landscape areas of the community. The Landscape Master Plan shall be guided by the following features:

- 1. The landscape design treatment for Placer Vineyards shall consider the context, historical land use, and ecology of the region. Design elements consistent with the historic rural image of the area including use of stone, wood, timber arbors and trellises, and rural fencing types are encouraged.*
- 2. Use landscape materials, trees, shrubs, and ground cover that are native to the area, have low water use, and fit into the local environmental conditions. Refer to Appendix B, "Recommended Plant List."*
- 3. Use colorful ornamental landscaping as accents at entryways, at special park and open space areas, and as image elements in the overall landscape.*
- 4. The streetscape designs described in the following sections of this chapter and the conceptual design figures that accompany them will be further refined and modified during the preparation of the Landscape Master Plan.*

Policy 6.2 View Protection.

Landscape elements shall be designed to create vistas and frame views of important community features, such as natural site features, significant buildings, or landmarks.

Policy 6.3 Natural Resource Preservation.

Where possible, open space areas shall interconnect with the oak woodlands, grasslands, wetlands, and other natural resources in the Plan Area.

Policy 6.4 Recommended Plant Palette.

Appendix B of this Specific Plan contains a list of plants recommended for use in Placer Vineyards. This list should be used when designing open space, landscape buffer corridors, streetscapes, gateways, and parks. Plants similar to those listed in the table may also be used, subject to County review and approval.

6.2.2 STREETSCAPES

The streetscape is one of the major visual elements that can help tie the community together in a consistent theme and quality of experience. Streetscape design is concerned with the design of the "public realm" or the area that stretches from the public streets to the building face. Streetscape design includes the selection of landscaping along the street and front yards and includes pedestrian amenities along the public right-of-way such as seating, lighting, and pedestrian furniture. Streetscape design guidelines apply to street corridors and along landscape buffers, medians, and gateways.

Trees are a major component of the streetscape design and livability of the Placer Vineyards community. Trees provide an attractive setting for walking and bicycle use, shade the sidewalks, street surface, and adjoining buildings; and improve the air quality and overall environment of the neighborhood. The streetscape design within the Plan Area is intended to provide a unified design and character for the project.

The guidelines presented in this section complement the roadway design standards found in Chapter V.

Goal 6.5 Create an attractive and inviting setting for the "public realm" that supports an active and vital mixed-use community. Features will include trees, shrubs, and ground cover planting, as well as trails, paths, monument signs, and other architectural amenities.

Goal 6.6 Design streetscapes along thoroughfares and arterials with a more spacious character to the public realm, consistent with the size and width of the street corridor, the volume and speed of traffic, and the amount and type of pedestrian activity on the street.

Goal 6.7 Create a continuous canopy of tree coverage throughout Placer Vineyards that establishes a strong and attractive community identity and provides shade.

Policy 6.5 Landscape Corridors on Major Streets.

Street landscape corridors will be developed along thoroughfares and arterials (i.e., Watt Avenue, Dyer Lane, and 16th Street; see Figures 6.1 and 6.2) and along major collector streets.

- 1. These parkways are wide and heavily landscaped with double rows of street trees, ground cover, and flowering accent vegetation.*
- 2. Walkways shall be designed as joint-use pedestrian and bicycle paths running through the parkway.*

3. *The streetscapes shall also provide linkages to neighboring open spaces, parks, schools, and major activity nodes, allowing for pedestrian and bicycle circulation as well as for other passive recreational and educational opportunities.*
4. *The corridors will have walks and trails separated from the streets. Landscaping will be expanded at entries to the development and blended in with the adjoining park sites and open space areas.*

Policy 6.6 Street Landscape Corridor Design.

Street landscape corridors shall be designed with different plant palettes to give a unique character to the streets. Landscaping along roadway corridors shall provide a unifying element to a neighborhood. Refer to Appendix B for a list of recommended streetscape trees, shrubs, vines, and ground covers, and Figures 6.1 through 6.2 for sample street corridor designs for Base Line Road and Watt Avenue.

1. *Thoroughfares: Base Line Road, Watt Avenue and Arterial Streets: Dyer Lane and 16th Street. Thoroughfares (see Figures 5.3-A, B) will have a 50-foot landscaped setback buffer and 20-foot median throughout the project area. Arterial streets (see Figure 5.3-D and E) will include a 35-foot landscape setback on both sides of the street and a 14-foot planted median. Landscape buffers will include earthen berms and plants, deciduous trees, evergreen trees, and drought-tolerant shrubs and ground covers. Berms and plants, such as 3-4-foot tall hedges, should be used to screen parking areas and/or sound walls.*

For thoroughfares and arterial streets, a row of trees for shade with drought-tolerant shrubs and ground covers will be planted in the road medians. Low drought-tolerant plants shall be installed as temporary landscaping in the future BRT right-of-way on Watt Avenue. Deciduous and evergreen canopy trees will be planted between curbs and sidewalks, with evergreen screen trees placed between sidewalks and residential walls. Landscaping shall provide for a consistent use of plant materials for the thoroughfares and each arterial street.

Unless recycled water is used for irrigation, irrigated turf will be limited to 25% of the total landscape corridor area. Turf areas will be limited to accent areas near intersections, project signage, buildings, and areas between the back of the curb and sidewalks.

2. *Major Collector Streets: Palladay Road, Tanwood Avenue, 14th Street. Select collector streets, as shown in Figure 5.3, Section F, will be landscaped with a 20-foot landscape setback area. A single, large deciduous canopy tree or other thematic landscape combination should be established per street. The same tree or repetition of trees will be planted within landscaped areas on both sides of the street at regular intervals for the entire road segment. In addition, other planting will consist of drought-tolerant ground covers, vines, and shrubs to screen walls or fences that abut the collector street landscape areas.*
3. *Minor Collector Streets and Local Streets. Local streets will be landscaped with a single, large deciduous canopy tree planted at regular intervals (at approximately 25-30-foot intervals or at a distance that provides appropriate spacing for the type of tree selected) on both sides of the street for the entire length of the road. In addition, other planting will consist of ground covers, vines, and shrubs to screen walls that "side-on" to local streets (see Figure 5.3, Sections G, H, R1 through R3, R5, and R6 for typical street cross sections).*
4. *Residential Alleys. All unpaved alley setback areas shall be landscaped.*

Policy 6.7 Street Tree Design.

Street tree planting throughout Placer Vineyards will vary depending on the type and size of each street and the desired character of the individual neighborhoods. Street tree conditions will serve as visual screens and buffers from development.

1. *In general, street trees shall be planted at regular intervals (approximately 25-30 feet apart), depending on the type of tree selected, to create a shaded canopy along the pedestrian travel way (see Appendix B for a recommended plant palette).*
2. *Street tree planting shall also be modified to incorporate existing trees and native oaks, when possible.*
3. *Street trees along major roadways shall be larger, scaled in relation to the street corridor and automobile travelers. Along thoroughfare and arterial streets, the dominant tree specified will be used 70% of the time.*
4. *Neighborhood streets shall be designed to include one row of large deciduous canopy trees.*

Figure 6.1 Conceptual Base Line Road Landscape Corridor

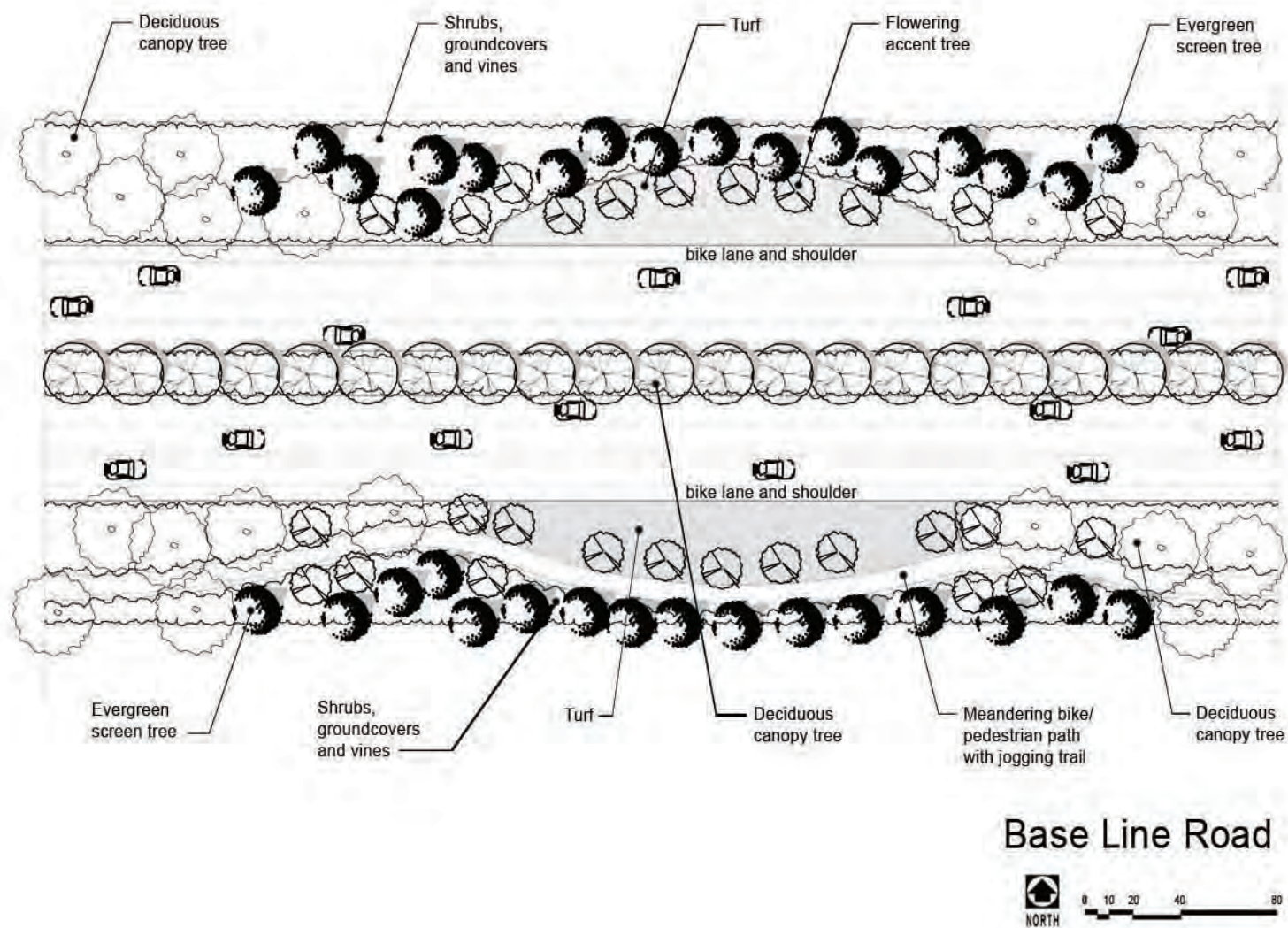
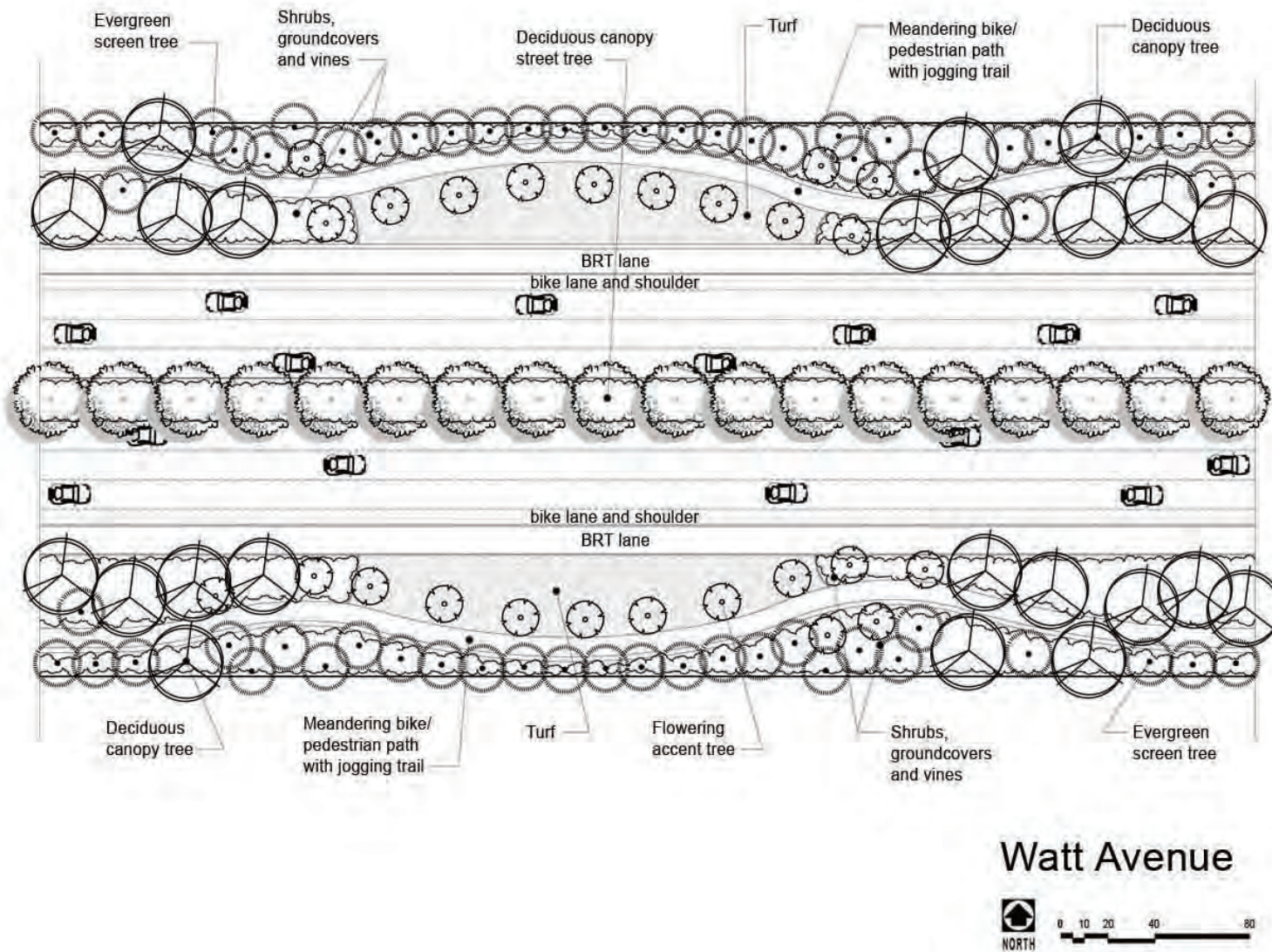


Figure 6.2 Conceptual Watt Avenue Landscape Corridor



Street Tree Design Guidelines

1. The size and spacing of street trees can help identify the street classification and function. In general, thoroughfares and arterials should be landscaped with larger canopy street trees and spaced accordingly. Collectors should be planted with different, smaller tree species, and local neighborhood street trees should be planted with yet another tree type.
2. Deciduous canopy trees and low ground cover should be placed in landscaped areas between the curb and sidewalk, while evergreen trees, shrubs, and vines should be located in landscaped areas between the sidewalk and walls or buildings. Trees should be planted with sufficient spacing to create full coverage and to allow for mature growth.
3. Street tree selection and plant locations should vary along the major thoroughfares, such as Watt Avenue and Dyer Lane, to create a naturalistic feel to the street corridors.
4. Tree selection should reflect the unique character of each neighborhood or street. Trees should be planted to ensure the spatial continuity of the street. Street trees should provide a large canopy with shape as well as color, variety, and accent.
5. Accent trees with color and seasonal flowering should be planted in clusters that exhibit interest and highlight intersections, entry points, or important destinations.
6. Street tree selection should reflect the environmental characteristics of the region. Low maintenance and drought-tolerant species are recommended.
7. Selected street trees species should have the ability to thrive in urban conditions where tree roots are often impacted by sidewalks and other obstacles such as underground utility lines and vaults.

Sidewalk and Street-Side Trail Design Guidelines

General guidelines for sidewalks and trails along major streets include:

1. Highly visible and lighted crosswalks should be located at all major intersections. Alternative surface materials are encouraged at crosswalks;
2. Sidewalks should have a minimum clear path of 5 feet in Placer Vineyards for pedestrian and wheelchair access;
3. Streetscape elements such as seating, benches, bike racks, signage, fire hydrants, news racks, bus shelters, lighting, planters, trees, and utility poles and boxes should be located in landscape lots or sidewalks. It is desirable to combine these elements when feasible;
4. Privately maintained areas adjacent to sidewalks should enhance the public space. Materials used for walls, fences, and pavement should be coordinated with and complement the streetscape design; and
5. Streetscape elements and paving materials should be of high quality to minimize maintenance. The quality of material and surface pattern contributes to the pedestrian experience.

6.2.3 COMMUNITY GATEWAYS

The gateways into the community help define separate and distinct areas of the project, while providing for a sense of arrival. Placer Vineyards is designed with a series of community and neighborhood entry points. The entryway concept provides a hierarchy of entryway experiences and, when coordinated with the street system, will help orient visitors and community residents to the site. Entryway designs should complement the overall image and style of Placer Vineyards community, and will be further defined in the *Landscape Master Plan*. Placer Vineyards will incorporate primary entries at major arterials throughout the project and minor entry points at other streets (see Figure 6.4).

Goal 6.8 Create attractive gateway features that help establish a distinct identity for the Placer Vineyards community and its neighborhoods and helps orient visitors to the site.

Goal 6.9 Use a consistent application of elements such as landscaping, signage, fencing, and lighting at key entrances to the community.

Policy 6.8 Community Gateway Signage.

Community gateway signage will consist of monument signs set in medians or on one or both sides of the street. These monument signs shall be located outside the right-of-way when they are proposed along the sides of streets and may be incorporated into walls or other architectural features to emphasize the transition. A complete signage program design for Placer Vineyards will be developed as a part of the Landscape Master Plan (see Policy 6.1).

Policy 6.9 Minor Entries.

Minor entries shall be located outside the right-of-way at intersections to the internal street system to identify and mark the transition into residential neighborhoods in Placer Vineyards. Entries are small-scale landscape or architectural features, signs, or monuments at the street corners that may include special lighting, flowering plants, or public art. These features are smaller in scale than community entries and serve as landmarks that give special identity and character to each residential neighborhood or block.

Policy 6.10 Community Gateway Theme.

All entries shall include thematic architectural or landscape design elements that incorporate a special landmark feature or public art to identity destinations in the community, representative of the Placer Vineyards character. Public art shall be subject to Design/Site Review approval.

Policy 6.11 Northern Community Gateways and Entries.

The primary northern entries will be provided on Base Line Road at Dyer Lane, 16th Street, Watt Avenue, and East Dyer Lane. Minor entries will be located at other additional collector roadways intersecting Base Line Road, as indicated in Figure 6.3.

Policy 6.12 Southern Community Gateways and Entries.

South of the project site, primary entries will be located along the Sacramento/Placer County Line at 16th Street, Tamwood Avenue, and at Watt Avenue where it intersects with Dry Creek (see Figure 6.3).

Policy 6.13 Lighting of Entryway Signs.

Lighting of entry signs shall consist of either indirect lighting with the light source at ground level or internal halo-type lighting. In both cases, the lighting source shall not be visible from traveled roadways.



Entries may be identified using landmark features or public art

Figure 6.3 Community Gateway Site Location Diagram



Design Guidelines for Community Gateways and Entries

1. Entries should be designed to represent the character of Placer Vineyards.
2. Entries should be treated with similar materials, colors, and forms.
3. Entries should be designed to highlight the transition into Placer Vineyards with taller vertical elements.
4. Entry walls should be designed as low-profile monument signs.
5. The ground plane entering into the Placer Vineyards neighborhood core should be highlighted from the street with colored and textured paving.
6. Landscape materials, oak trees, and other planting should be formal groupings, to reinforce the signage and entryway features.
7. Landscaping should be carefully placed to avoid obscuring signage.
8. Up-lights and halo-lit signs and monuments should be integrated into entryway features.
9. Primary entry signs are not intended to be used for individual business advertisement.
10. Entry signs will be designed not to interfere with vehicle sight distances.



Community Gateway monument sign design



Use ground cover, shrubs and other plants to integrate monument signs into the overall site landscape



Minor entryway sign used to identify a residential neighborhood or district

Design Guidelines for Minor Entries

1. Minor entries should be designed with a consistent visual theme using similar materials, landscaping, lighting, and architectural forms.
2. The minor entries should be incorporated into the landscape areas outside the street rights-of-way. Features may include landscaping, signage, lighting, decorative walls, and other architectural forms that provide transition.
3. Designs should incorporate pedestrian streetscape elements and landscape materials that represent the area's urban, mixed-use character.
4. Surface textures and colored paving materials may be incorporated into the ground surface.
5. At the entry into mixed-use areas, land uses and building corners should exhibit a more active and pedestrian retail environment. Within the town center and commercial areas, entries should incorporate larger building setbacks, corner entry plazas, and storefront windows and doorways.
6. Minor entry signs should be designed to be low-profile, monument-type signs that do not interfere with vehicle sight distance.
7. Minor entry signs are not intended to be used for individual business advertisement.

6.2.4 SIGNAGE SYSTEMS

Signs and landmark features located throughout the Plan Area will help to establish a coordinated and consistent quality, character, and image for Placer Vineyards and aid residents and visitors in navigating through the community.

The intent of creating a system of permanent signs is to convey information regarding the names of streets, neighborhoods, particular locations, and community facilities. Permanent signs include community entry signs and features, neighborhood signs and entry landmarks, street signs, directional signs, and business identification signs. The complete signage program design for Placer Vineyards is not addressed in this Specific Plan but will be developed for review and approval by Placer County separately in more detail at a later date.

Goal 6.10 Create a coordinated and consistent quality of signage for the Placer Vineyards community.

Policy 6.14 General Sign Requirements.

All signage shall be subject to the approval of a sign permit by the Placer County Planning Department and shall conform with the sign standards of the Placer County Zoning Ordinance (see Sections 17.54.170 through 17.54.200), unless this is superseded by the adoption of a separate signage program for Placer Vineyards.

6.2.5 LIGHTING

Lighting throughout the Plan Area is an integral part of the overall community image and character. In addition to reinforcing the character of the town center, village centers, parks, and the streetscape, lighting helps to increase the safety and security of residents and users. The lighting design for Placer Vineyards is guided by the following goals and policies.

Goal 6.11 Provide sufficient lighting to increase the safety and security of residents and visitors to Placer Vineyards on its streets, parks, and pedestrian pathways. Provide lighting that is attractive and compatible with other design features in the community.

Goal 6.12 Maintain the spirit and intent of the Dark Skies standards (the ability to view the night sky) by protecting against glare and excess lighting; providing safe roadways for motorists, cyclists, and pedestrians; and promoting efficient and cost-effective lighting.

Policy 6.15 General Lighting Requirements.

Lighting shall be designed and located to meet minimum, necessary ambient light levels for any given location consistent with public safety standards.

1. *Lighting shall occur at intersections, areas of major pedestrian activity, and building entries, and generally shall be minimized elsewhere.*
2. *Street lighting shall be directed and regularly spaced, and shall project light down to the road surface and away from the building.*
3. *Decorative pedestrian-scaled lighting fixtures shall be used in the town center, village centers, commercial centers, parks, and other major pedestrian activity areas.*
4. *The selection and design of lighting fixtures shall minimize glare and excess spillage onto neighboring properties. All street lights shall utilize cut-off fixtures to minimize visibility from adjacent areas of the community and public use areas.*

5. *No lighting shall blink, flash, or be of unusually high intensity or brightness.*
6. *Lighting fixtures shall be energy efficient and provide a balance between energy efficiency and pleasing light color and coverage. Avoid low-pressure sodium fixtures. Acceptable light sources include mercury vapor, metal halide, and similar sources.*

Policy 6.16 Street Lights.

Street lights on Base Line Road and Watt Avenue shall be simple cobra-head light fixtures, located particularly at major intersections. Street lights for collectors and local streets should be decorative fixtures that are compatible with character of the Placer Vineyards streetscape. The selection of street lights for Placer Vineyards will be addressed in the Landscape Master Plan.



Cobra-head street lights shown in the background

COMMUNITY DESIGN

1. **Color:** The color of street light poles and fixtures shall be consistent throughout the community. All specified metals shall utilize anodized coatings, rather than painted colors.
2. **Materials:** Street light poles shall be either metal or wood fixtures.
3. **Town Center:** Street lighting in the town center shall consist of ornamental street lights flanking the roadways. Street lights shall be located in parallel pairs along the length of the street, spaced to provide illumination levels of 0.5 footcandle, and shall not exceed 14 feet in height.
4. **Village Centers:** Village centers shall use a design in the same design family as the ornamental street lights in the town center; however, each village center shall utilize a light fixture designed to give the center a unique character and identity.
5. **Base Line Road and Watt Avenue:** Street lights on Base Line Road and Watt Avenue shall be simple, single- or double-arm, cobra-head lights, spaced as required to maintain illumination levels of 1.75 footcandle, and shall not exceed 40 feet in height.
6. **Arterial Streets:** Street lights on arterial streets shall be selected decorative fixtures, spaced, as required, to maintain illumination levels of 1.0 footcandle on alternating sides of the street, and shall not exceed 25 feet in height.
7. **Collector and Local Residential Streets:** Street lighting on collector and local streets in residential neighborhoods shall be ornamental or decorative street light fixtures, spaced, as



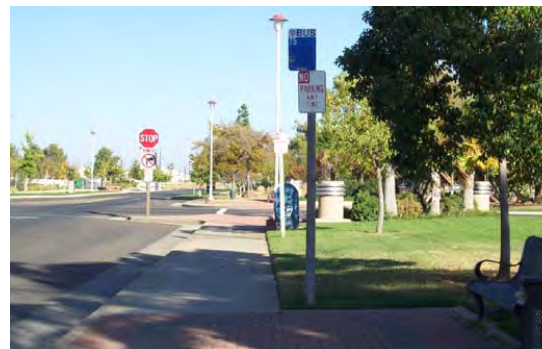
Ornamental streetlights in the town center



Banners on decorative, arterial streetlights



Residential streetlight



Streetlights and bollard lights along a pedestrian path.

required, to maintain illumination levels of 0.5 footcandle on alternating sides of the street, and shall not exceed 14 feet in height.

8. **Neighborhood Entries:** Street lights at neighborhood entries shall consist of ornamental or decorative fixtures flanking the roadway, located in pairs, and shall not exceed 14 feet in height.

Policy 6.17 Pedestrian Oriented Lighting.

Pedestrian oriented lighting will be provided within the town center, village centers, commercial centers, parks, and major pedestrian activity areas. Pedestrian lighting shall consist of smaller pole fixtures, spaced to maintain illumination levels of 0.5 footcandle, and shall not exceed 14 feet in height.

Design Guidelines for Pedestrian Oriented Lighting

1. Bollard light fixtures are also recommended along pedestrian paths of travel as an alternative to pole-mounted light fixtures.
2. With the exception of sports lighting, wood poles are recommended in parks and open spaces and along major pedestrian paths, in keeping with the more rural and rustic theme for Placer Vineyards.
3. Specialized pedestrian lighting should be used to distinguish differences in individual neighborhoods and areas within Placer Vineyards.
4. Pedestrian lighting should be incorporated into the landscape design along major pedestrian paths of travel at appropriate intervals to provide lighted coverage.

5. Bus shelters, telephone kiosks, and other site furnishings should be adequately lighted for security and functionality. Light sources shall be included in the structure of the furnishings.
6. Pedestrian lighting should be durable and easily maintained and of sufficient intensity to provide lighting without spilling over onto road rights-of-ways and adjoining properties, or conflicting with street lighting.

Policy 6.18 Recreation Areas and Athletic Facilities.

Lighting for athletic fields and court facilities shall be designed to minimize illumination and spillage onto neighboring uses and residential areas. Athletic facility lighting shall be provided only in the two community park facilities and the town center recreation center. Mercury vapor or metal halide lamps, mounted 30-50-feet high, are recommended.

Policy 6.19 Public Parking Lot Lighting.

Parking lot lights shall be no higher than necessary to provide efficient lighting of the parking areas and shall not exceed 25 feet in height for larger parking lots, including the base. Illumination levels shall be maintained at 1.0 footcandle. Spacing between light poles shall be determined by the required level of illumination.

1. The fixture design style and colors shall be compatible with the architectural design of the site.
2. Within the town center and village centers, the design and color of parking lot light fixtures, shall be consistent with the design of street lights.

Policy 6.20 Landscape Lighting.

Landscape lighting shall be limited to important landscape areas, entryway features, signage, or pedestrian areas.

1. The design of landscape light fixtures shall be hidden from direct view unless designed as an integral part of the area's design.



Parking lot lighting design examples



Bollard light fixture integrated into site landscaping

2. The light source shall be shielded from direct view at night, with the emphasis on the object or area being lighted. Up-lighting and mood lighting in trees and landscaping can be effective design feature for special entries, signs, water features, and landmarks.
3. Landscape light fixtures shall be durable and easily maintained. For ease of maintenance, landscape light fixture types and lamps shall be consistent with landscape design and street lighting for each neighborhood.

Policy 6.21 Service Areas and Security Lighting.

Lighting in services areas shall be designed to avoid spillover onto adjacent properties and activity areas, and shielded from direct view of the light source at night.

1. Building-mounted flood light fixtures shall not be used unless the light source is completely shielded from public view.
2. Low-pressure sodium fixtures or other lighting types that contrast excessively with lighting in the surrounding use area shall be avoided unless completely shielded from public view. Mercury vapor, metal halide, and similar fixture types are permitted.

Policy 6.22 Building Identification and Street Number Lighting.

Lighting of building identification and street numbers on buildings shall be used to help locate buildings during evening hours.

1. Lighting of street identification or numbers shall be internally lit, back lit, or externally lit. External spot lighting is preferred.
2. Street number lighting shall be subdued and not contrast excessively with the surrounding area lighting.

COMMUNITY DESIGN

3. *Numbers and lighting shall be conveniently placed near the building entry and shall be visible from the street and adjoining parking lots.*
4. *Buildings placed back from and not visible from the street may use freestanding lighted monuments or small pedestrian pole lights with mounted street numbers to identify the street address of the buildings from the street.*

6.3 ACTIVITY CENTERS

The centers of activity are the community gathering places that are specially designed to serve the Placer Vineyards community. See Figure 6.4 for the location of activity centers. Activity centers include the town center (see Section 6.3.3), the village centers (see Section 6.3.4), the regional commercial centers along Base Line Road (see Section 6.3.5), and the neighborhood commercial centers (see Section 6.3.6).

Special treatment is given to the design of these mixed-use neighborhood sites, with emphasis placed on creating a safe and pedestrian friendly setting with easy and convenient access by multiple means of transit. Commercial and other non-residential properties located within the centers of activity shall require Design/Site Review. All buildings and improvements proposed within the activity centers are subject to approval of a Design/Site Review agreement as outlined in Section 7.52.070 of the *Placer County Zoning Ordinance* and to Appendix A, “Land Use and Development Standards.”

6.3.1 GENERAL GOALS AND POLICIES FOR ACTIVITY CENTERS

- Goal 6.13** Design for a variety of activity centers that serve their respective functions and add value to the communities where they exist through the provision of jobs, services, and economic activity.
- Goal 6.14** Provide a range of housing close to commercial centers with paths that connect them.
- Goal 6.15** Create attractive and comfortable outdoor, pedestrian activity areas that are adjacent to buildings, entries, and along major commercial streets.

Policy 6.23 Pedestrian Orientation.

Design elements that accommodate pedestrians and cyclists shall be equally treated or take precedence over elements that primarily accommodate automobiles, especially in the town center, village centers, neighborhood centers and access areas leading into parks, schools and other public facilities. Retail centers and commercial areas shall be designed to provide maximum pedestrian accessibility.

Policy 6.24 Density within Centers of Activity.

The town center, village centers, and Base Line Road commercial corridor should be established at sufficient densities to support express bus transit from Placer Vineyards and other neighborhood areas in the region.

- Goal 6.16** Design parking areas that are in scale with and are visually subordinate to the developments they serve.

Policy 6.25 Parking Lot Shading.

New parking lots serving non-residential developments shall include tree planting designed to result in 50% shading of parking lot surface areas within 15 years. These shading requirements shall apply to all impervious surfaces on which a vehicle can drive including parking stalls, drives, and maneuvering areas within the property. Until such a time that Placer County adopts a Parking Lot Shading Ordinance, the County shall use the City of Sacramento Parking Lot Tree Shading Design and Maintenance Guidelines, June 17, 2003 edition to implement these requirements.

- Goal 6.17** Design buildings whose architectural character contributes to the vitality and attractiveness of the districts, neighborhoods, shopping centers, and work places in the community.

- Goal 6.18** The heights of buildings shall be related and proportional to the activities and surrounding context in which they are located.

Policy 6.26 Building Heights.

Taller buildings will be located along the major commercial nodes, at the town center and within the office and commercial areas along Base Line Road. Lower story buildings and less intense development will be located adjacent to existing neighborhoods in the Special Planning Area to provide appropriate transitions to more intense development at the center and along the major transportation corridors in the Plan Area.